

**AGENDA**  
**CITY OF STEVENSON SPECIAL COUNCIL MEETING**  
**March 05, 2019**  
**6:00 PM, City Hall**

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**1. CALL TO ORDER/PRESENTATION TO THE FLAG:** Mayor to call the meeting to order, lead the group in reciting the pledge of allegiance and conduct roll call.

**2. OLD BUSINESS:**

- a)** **Interim Zoning Control** - The City Leadership Team presents the letter from the Mayor regarding the veto of Ordinance 2019-135, information from "The 20 Ingredients of an Outstanding Destination," and the attached memo and revised ordinance 2019-1138 regarding interim zoning changes for the commercial zone.

MOTION: To approve Ordinance 2019-1138 regarding interim zoning controls for the downtown area and key commercial corridors within the City of Stevenson, replacing existing development moratoria and repealing ordinances 2018-1127, 2018-1130 and 2019-1139.

**3. ADJOURNMENT** - Mayor will adjourn the meeting.



# CITY OF STEVENSON

7121 E Loop Road, PO Box 371, Stevenson, WA 98648

3/4/2019

To the Stevenson City Council,

In 2013 the City adopted a Comprehensive Plan which lists “consider allowing new single-family development in the downtown area as conditional uses”.

At the end of 2012, several of Stevenson’s downtown shareholders gathered together to view a webinar series entitled “20 Ingredients of an Outstanding Downtown” provided by Destination Development International and its enthusiastic CEO Roger Brooks (Attachment 1).

I was a part of that group and I have made improving the economic position and vitality of downtown Stevenson a key goal of mine in the years since then. I am proud to see how far we have progressed in this time. We have:

- Formed the Stevenson Downtown Association, a Main Street Organization capable of diverting ~\$100,000 per year from state funds for our own use locally.
- Invested more than \$1mil in streetscape improvements to Cascade Avenue and the Quiet Zone silencing more than 40 train horns at Russell Street.
- Designed and are preparing to construct streetscape improvements to middle and lower Russell Street.
- Designed and installed wayfinding signage.
- Designed and are preparing to construct pedestrian improvements and water access to the riverfront between Russell and Leavens streets.
- Designed and are searching for funding for the Courthouse Plaza project for a public gathering place and public restroom which will be the envy of communities across the region.

The 7<sup>th</sup> ingredient from Roger Brooks’ webinar series involves pioneering investors with patient money. Through the City’s investment—and the patience to see those investments pay off—we are modeling the behavior we want to see from others. In doing so, we have prepared fertile soil for the growth of economic activity downtown. We are beginning a planning process to better identify what we need to nourish the growth we seek. Unfortunately, some of the plants taking root will detract from the goals we hold for downtown. Through the moratorium the Council adopted as Ordinance 2018-1130, you all saw the opportunity and need to address those conflicting uses before they take hold. I commend you for the action. In Ordinance 2019-1135, though, you fundamentally undermine the original moratorium’s purpose which will forever limit what our downtown can become. I am not willing to see that happen. I am willing to remain patient in the face of this political pressure.

It is for these reasons I have vetoed the City Council’s 3 ayes to 2 nays vote to adopt Ordinance 2019-1135.

With optimism for our future,

Scott Anderson

Mayor, City of Stevenson



# The 20 Ingredients of an Outstanding Destination

- HANDOUT -

# The 20 Ingredients of an Outstanding Destination | Handout

## Where did this come from?

For more than 30 years the DDI team has worked around the world helping communities become outstanding destinations for investment, new business opportunities, and as a place to live and visit. Coming from the tourism industry, we discovered an interesting fact: The number one activity of visitors in the world is shopping, dining and entertainment in a pedestrian-friendly, intimate setting – your downtowns. This is where 80% of all non-lodging tourism spending takes place. That's right: 80%.

Is it any wonder that Disney has built Downtown Disney right next to its theme parks? It's to capture that visitor spending. Understanding this changed our business – we realized that downtowns are critically important to any tourism program and that downtown and destination marketing organizations should be joined at the hip.

Digging a little further, we found a couple more interesting facts:

1. If you don't hang out in your own downtown, neither will visitors. Visitors go where residents go. If that's somewhere besides downtown, that's where visitors will go as well.
2. Overnight visitors spend the night where there are things to do after 6:00 pm. In fact, 70% of all consumer retail spending, including that of visitors, now takes place during the evening hours. Are you open?

But even with this information, a big question loomed: What does it take to create a downtown where visitors AND locals would spend time and money? We decided to find out.

We developed a list of criteria that included nearly 80 items: from store hours, to parking limits, to business mix, wayfinding signage and marketing programs.

We then surveyed 400 successful downtowns and downtown districts (big and small) throughout the U.S., Canada, and Western Europe. Using the 80-item list we found the 20 most common ingredients that led to their success.

Few downtowns had all 20 ingredients, but those with 17 or more got an "A" from our research and were clearly successful destinations. Many had 15 or 16 and those earned a "B," and those with 13 or 14 earned a "C." After working with nearly a thousand communities and surveying hundreds of others, we found that more than 600 of those did not meet the qualifications to earn a "C," and were not nearly as successful.

Additional criteria to determine success was then included:

- The downtowns or districts had to have at least a 97% or higher occupancy rate in the core district or area being surveyed.
- The lease rates had to be "market rate" or higher compared to similar cities and towns in the state, province, or region.
- The turnover had to be less than 5% per year. Revolving door downtowns are not sustainable in the long run.
- They downtowns or districts are seen as the lifeblood of the community. Full of activity and life.

There are 19,500 cities and towns in the U.S., and of those, 18,500 have populations of less than 40,000 residents. In Canada there are more than 8,000 cities, towns and villages, of which more than 7,500 have smaller populations. And the majority of these cities and towns have struggling downtowns. We are in an age when people want to spend time in vibrant downtowns more than ever before. Yet because many downtowns have property owners who do not work together with a common vision, many of whom are absentee owners, or they have regulations that stifle successful retail or other dysfunctional qualities, developers are now building new "downtowns" – lifestyle retail centers. These are designed to mimic the traditional downtown of yesteryear. Virtually all of them use these same 20 ingredients to ensure their success.

As you go through the 20 Ingredients of an Outstanding Downtown, see how your downtown or district stacks up. If you have 13 or more of these ingredients, wonderful! Please let us know! We are always updating our research, looking for great case histories, and would love to share your story with others.

For those of you working hard to turn your downtown into an outstanding destination, we hope these ingredients can become your "Action Plan" so that your community can enjoy the benefits of an enjoyable, economically healthy and vibrant downtown. We would love to add your community to our list of Outstanding Downtowns.



## The Property Owners List



### 1. They all begin with a plan

Successful communities start with a plan. Bring together your partners and stakeholders, and work together so that everyone's efforts are towards common goals and a defined direction.

- Every community has different features, so each community needs its own special Branding, Development & Marketing Action Plan.
- Identify what sets your community apart from everyone else. Make that the focus of your executive summary and your detailed vision statement. That will be your strongest sales tool.
- Your Action Plan needs to include your brand and brand promise, product development plan, and marketing plan.
- Your Action Plan needs to be a detailed "to-do list."



### 2. They defined a strong brand and retail focus

Branding is perhaps the most misunderstood concept in the world, yet here we are smack dab in the middle of the "Era of the Brand." Outstanding destinations have a strong brand and a successful, vibrant retail core.

- Brands are perceptions
- Brands are built on product
- Brands are earned: Good or Bad
- Brands are built using public relations and word of mouth
- Brands must be experiential
- Branding is the art of differentiation
- Jettison the generic

# The 20 Ingredients of an Outstanding Destination | Handout



### 3. They orchestrated recruitment of “critical mass” or “clustering.”

Successful downtowns need to have a critical mass of like businesses. This would include a minimum in three lineal blocks:

- **Ten places that sell food:** soda fountain, coffee shop, bistro, café, sit-down restaurant, wine store, deli, confectionery.
- **Ten destination retail shops:** galleries, antiques (not second hand stores), collectibles, books, clothing, home accents, outfitters, brand-specific businesses, garden specialties, kitchen stores, cigars, etc.
- **Ten places open after 6:00 pm:** entertainment, theater (movies, performing arts), bars & bistros, specialty shops, dining, open air markets, etc.



### 4. They each have “anchor tenants”

An anchor tenant is your primary lure – what makes you worth a special trip.

Like every mall, every town relies on anchor businesses to attract customers, and all the other businesses benefit.

To be a successful community, you must have at least one or two anchor businesses, ones that people would drive an hour – or more – to visit.

Always promote your anchor tenants – your primary lures. Create a “best of” brochure for your community that highlights your very best attractions and businesses.



### 5. Lease agreements included defined operating hours and days

- 70% of all consumer spending (both locals and visitors) takes place after 6:00 pm. Are you open?
- People spend the night where there are things to do after 6:00 pm. Visitors don't like sitting in a hotel room after dinner watching TV.
- Conferences and conventions are booked around things to do AFTER the meetings adjourn that day.
- The majority of the businesses must be open after 6:00, not just a few. If half the stores in the mall closed at 6:00 rather than 9:00, would you go to the mall after 6:00?
- Start by staying open on Friday and Saturdays until 7:00 pm the first year, then add Thursday, then Wednesday.
- Bring nighttime music and entertainment downtown to provide incentives for people to go.



# The 20 Ingredients of an Outstanding Destination | Handout



## 6. People living and/or staying downtown

- The reinvention of downtowns also includes residential upper-story development: condos, loft apartments, downtown hotels and a business mix to support them.
- Residential drives retail. What comes first? A downtown that people will want to live in. This includes businesses open AFTER 6:00, entertainment, all in a pedestrian-friendly, attractive downtown setting – a “Third Place:” the place we want to live and hang out.
- Hotel development downtown is another great mix that provides “new” customers nearly every day. Hotel development helps restaurants and destination retail shops flourish.

## 7. Pioneers with patient money were convinced to invest

- Every downtown revitalization effort requires property owners with patient money, the will to “make a difference” and the ability to think long-term.
- Reduce rental rates until you develop the critical mass so that key merchants can stay alive until you become a destination downtown.
- Incentives need to happen from the property owners in terms of rent abatements, reduced lease rates, and a focus on the tenant mix: the critical mass.
- It takes one-third of the property owners, working together, to “reinvent” downtown in terms of business mix, curb appeal, upper story living units, etc.
- Every downtown effort requires tireless pioneers and those who will champion the cause. Enthusiasm is contagious and is an essential ingredient for every revitalization effort.

## 8. They started with just one or two blocks - a “demonstration project”

Begin revitalization in a very small area – concentrate your efforts so they make a big impact fast – just one or two blocks.

Where to start? Where the property owners are most willing to help with the efforts.

Concentrate the focus of creating critical mass in those blocks. Work with property owners on lease agreements, choice of tenants, facades, beautification.

Timeline: Three years. The rest of downtown will see the results and follow along – driven by the market.

## The Public Sector List



### 9. Solving the parking dilemma

- Two-hour parking in a pedestrian-friendly downtown restricts spending.
- If you insist on two-hour parking, then identify WHERE the all-day or extended hour parking is located.
- It doesn't have to be free, but needs to be reasonable.
- Consider incentive parking programs:
  - Spend \$20 or more and get the parking free
  - Have local banks and businesses "sponsor" free-parking days or evenings
  - Reduce the parking fees the longer they park downtown
- Angle-in parking generates increased sales over parallel parking. Additionally you'll get a third more spaces in the same area.



### 10. Public washrooms

- The number one reason passers-by stop in a town is to use restroom facilities.
- Restrooms should be in the heart of spending districts. Once visitors get out of the car, you have a four-times greater chance of getting them to spend money.
- Make sure they are open 24-hours a day



# The 20 Ingredients of an Outstanding Destination | Handout



## 11. Development of gathering places

- Turn parks into plazas. Common ingredients include:
- Permanent home for an outdoor open market with permanent structures. Open air (Farmers) markets should be placed in the heart of spending districts.
- Interactive water features
- Multiple stages
- Pocket parks or small venue plazas
- Amphitheaters
- Trees and raised planters
- Public art
- Night lighting and walking areas
- Music

## 12. Creation of good first impressions: Community Gateways

- Look at all gateway signage: does it reflect well on downtown?
- Always place your gateway signs where you make the first, best impression.
- Signage at city limits should be directional signage to key downtown districts. For instance "Downtown Anywhere – 1 mile."
- Place gateways at your downtown or district entrances. These can span the street, include decorative crosswalks, pole banners and other identifiers creating a "sense of arrival."

## 13. Design, fabrication & installation of a wayfinding system

- Wayfinding should be decorative to fit the brand.
- It should include both vehicular wayfinding as well as pedestrian wayfinding signage.
- Never put more than five items on any one sign.
- Use 1" tall letters for every 12' of viewing distance.
- Use "identifier" kiosks or maps to showcase district boundaries and key amenities and/or attractions.
- Every community should develop and implement a signage plan and program: wayfinding, gateways and entries, billboards and marketing displays, amenity identifiers. Nationally, less than 5% of visitors stop at visitor information centers – IF they can find that.
- Wayfinding also educates local front-line employees of what you have to offer and where it's located.

## The Merchants List



### 14. A good first impression: downtown or district gateways

- Downtown gateways create a “sense of place” and arrival. They can also promote a sense of group pride among the merchants downtown, helping to foster group cooperation with beautification and marketing.
- They should always be attractive and reflect the ambiance of the town, promoting a feeling that you have arrived at a special destination.
- Decorative crosswalks are an excellent way to separate the district and increase pedestrian safety at the same time



### 15. 20/20 signage: retail signage rules & regulations

- Develop perpendicular or “blade signs” in core downtown districts.
- Typical guidelines: no lower than 7'; no higher than 9'; no wider than 42". Keep them consistent in height and width so you don't create sign clutter.
- Create a merchant-driven signage review committee. This would include retail signage, the use of sandwich boards, extension of window displays into exterior spaces.
- Businesses need to promote, on signage, their key “lure” more than the name of the business.
- Get rid of the “Closed” signs and instead use “We'll be open” signs.
- Never use “restrooms are for customers only” or “no public restrooms” signage. Instead, tell customers WHERE they can find restrooms.



## The 20 Ingredients of an Outstanding Destination | Handout



### 16. Sidewalk cafes and intimate surroundings

The biggest trend in successful downtowns is the creation of intimate surroundings:

- Narrower streets
- Wider sidewalks
- Street trees every 30' to 35'
- Buffers between sidewalks and traffic or parking
- Decorative crosswalks
- Decorative night-lighting
- Architectural lighting

Other common ingredients include:

- Water features
- Sidewalk cafes and exterior dining



### 17. They invested heavily in retail beautification

- Extension of window displays to exterior spaces, NOT outdoor merchandising. Folding tables, boxes of goods, shopping carts piled with goods, are NOT good examples of curb appeal and should be discouraged or not allowed.
- The most important element of curb appeal is the softening of the transition of building facades and the sidewalk, not curbside street trees and landscaping.
- Besides word of mouth, shoppers typically have only curb appeal to help determine whether or not a shop is worth visiting. Curb appeal can account for 70% of new visitor sales at restaurants, retail shops, and hotels and lodging.



### 18. They provide activities and entertainment: bring downtown to life!

Bring downtown to life!

- Open air markets should operate for at least three days a week, during at least a 24 week season (depending on location).
- Invite street musicians and street artisans on weekends.
- Recruit outside events into plaza areas.

# The 20 Ingredients of an Outstanding Destination | Handout



## 19. They gave downtown districts a name

Give downtown a name. This will make it a destination, as opposed to just a place. It will also get downtown on highway signage.

Popular downtown district names include:

- Pearl District in Portland, Oregon
- Bricktown in Oklahoma City, Oklahoma
- Gastown in Vancouver, British Columbia
- Pioneer Square in Seattle, Washington
- Gaslamp Quarter in San Diego, California
- Baker Street in Nelson, British Columbia
- The Rail District in Snoqualmie, Washington
- Bourbon Street, The French Quarter in New Orleans, Louisiana



## 20. The marketing is experiential: focus on activities, not buildings

- Perceptions create a brand in multiple ways:
  - Visual cues
  - The people and attitudes
  - Word of mouth
  - Publicity, social media, peer reviews
- Always sell a feeling: not buildings and the physical environment
- Visitors are far more interested in the things to do than in the location
- Develop an activities guide that promotes what there is to do in your community



## New 3-Part DVD Series

### The 20 Ingredients of an Outstanding Downtown

We surveyed 400 popular downtowns and downtown districts and found the 20 most common ingredients that led to their success. Learn how these ingredients can help your downtown achieve success too!

Price: \$295 USD | 108 minutes

[www.ddistore.com](http://www.ddistore.com)



## Destination Development International

For over 30 years, the award-winning DDI team has had a singular mission – to improve people’s lives by helping their communities become healthier, more attractive, and more economically vibrant. We believe that every community has the potential to become a thriving, desirable place for residents, businesses, and visitors.

Working primarily in the public sector with cities, counties, states, CVBs, destination marketing organizations, and provinces, DDI is renowned for its bottom-line, “make-something-happen” approach. This results in tremendously successful planning and implementation efforts.

### Contact Us

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## About Roger Brooks

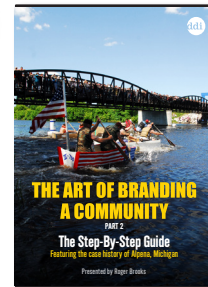
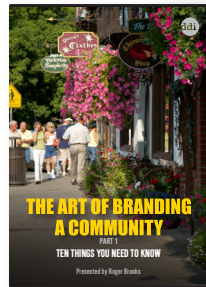
As CEO of Destination Development International (DDI), Roger’s expertise comes from having assisted nearly a thousand communities in their efforts to become better places to live, invest, establish a business, or visit.

The information is not just entertaining – it is clear, concise and it sticks. Known for telling engaging stories, Roger uses real-life examples shown in photos and video clips, some of them hilarious, all of them poignant. His bottom-line rules, tips, and ingredients leave attendees with rock solid action items they can implement today to make a difference tomorrow.



## VIDEO PRESENTATIONS

In this video series, host Roger Brooks provides you with bottom-line solutions using dozens of case histories and photographic examples that will help make you THE winning destination of choice. Available at [www.DDIStore.com](http://www.DDIStore.com)

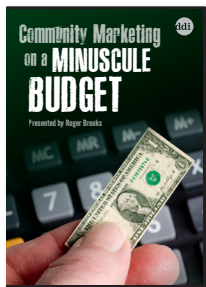


### The Art of Branding a Community Part 1 & 2

**Part 1:** In this eye-opening, must-see presentation by Roger Brooks, you’ll learn the ten things you need to do to successfully brand your community.

**Part 2:** In this fascinating presentation using Alpena, Michigan as a case history, you’ll learn HOW to successfully develop your own brand using the 13-step process that took nearly 30 years to develop and perfect. Best of all: This process has a 100% success rate.

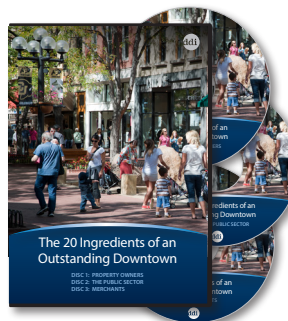
Price: \$295 USD | Part I: 63 min. Part II: 60 min.



### Community Marketing on a Minuscule Budget

Learn the seven marketing ideas you can implement today, for practically no money, that will dramatically increase local spending.

Price: \$95 USD | 70 minutes



### The 20 Ingredients of an Outstanding Downtown

We surveyed 400 popular downtowns and downtown districts and found the 20 common ingredients that led to their success. Learn how these ingredients can help your downtown achieve success too!

Price: \$295 USD | 108 minutes



### The Seven Deadly Sins of Community Marketing

A full 97% of community marketing is wasted. Learn the 7 deadly sins, how to avoid them, and how to make sure that 100% of your marketing is effective.

Price: \$95 USD | 72 minutes



# City of Stevenson

## Planning Department

(509)427-5970

7121 E Loop Road, PO Box 371  
Stevenson, Washington 98648

**TO:** City Council  
**FROM:** Ben Shumaker  
**DATE:** March 5<sup>th</sup>, 2019  
**SUBJECT:** Interim Zoning Controls

### Introduction

The City Council has been asked to consider adoption of interim zoning controls as an alternative to the several development moratorium and the status quo those moratoria were put in place to address. The proposed interim zoning controls have been developed based on "Downtown Stevenson: Draft Development Framework, Standards & Design Guidelines" from August 1999. This memo identifies key components and several updates to the draft presented in February requiring City Council discussion.

This memo will be supplemented by use of the white board in Council Chambers to better explain concepts and create common understanding.

### Proposal

The proposal is presented in landscape legal format. On the left side of pages, the 1999 proposal is preserved as originally written and on the right side of the pages new annotations are included where a change or explanation is necessary. For map pages, the annotations appear at the bottom of the page. This formatting is sometimes awkward, but staff felt it was the best way to 1) make changes to a document for which only a scanned pdf was available and 2) preserve the intent of the original while capturing the intended changes that better suite our era and our usage of this as a temporary regulatory approach.

### Key Pages for Review

#### Amendments to February Draft

Since the packet that was delivered to you in advance of the February meeting, the text on 17 pages has been changed and is available for your review. See:

Cover Page-Explanatory text added	Page DI-4-Page #, additional explanatory text	Page DS-5b-Page #, Note #s, Zone 2 general Street-Level Conditional Uses, Zone 3 Conditional Uses
Contents Page-Corrected section titles	PA-10a-Cross-reference added	
Page I-2-Design Guidelines text	PA-10b-Cross-reference added	
Page I-4-Bullet points added	Page DS-1-Typo corrected	Page DS-5c-Side yard access note added, Note #s, SFR note modified, Duplicate deleted
Page DI-0-Corrected section titles	DS-2-Page #s	
Page DI-2-Typo removed, explanatory text added	DS-4-Page #s	
	Page DS-5-Page #s	Page "DS-7" Incentives added
	Page DS-5a-Note #s,	

### Programmatic Components

The following conceptual components of the draft interim controls should be addressed by the City Council. All questions are written in a way that a "yes" answer means the Council can move on without contemplating amendments to the text. Dissent and disagreement is expected and encouraged despite the framing of the questions in this way.

Page I-2-Downtown Development Goals-The left side of this page describes the goals the City Council is trying to achieve with the adoption of these interim controls. Staff has kept the 5 goals as they were drafted in 1999 and added a goal to explain the increased plan area.

**DECISION POINT:** Does the City Council concur with these goals?

Page I-2 Design Standards vs. Design Guidelines-On the right side of this page it draws a distinction between standards (strict requirements) and guidelines (loose suggestions). There are 37 pages of the document devoted to guidelines. Only 10 are devoted to standards. The draft language gives the Planning Commission "wide latitude" to waive the applicability of guidelines to specific projects.

**DECISION POINT:** Does the wide latitude language capture the Council's intent during this interim period?

Page I-4 Minor Projects 1 - As originally drafted, the concept for minor projects needs additional clarification (distinguish between new construction, remodels, and general maintenance improvements). Instead of reworking the document to provide that clarification, staff drafted a fix that considers all projects as "Major" but provides an off-ramp for what 3 reasonable people (i.e., the currently non-existent "Design Liaison Committee") would agree is truly minor.

**DECISION POINT:** Does this Major Project plus "exit ramp" fit the Council's view of how projects should be reviewed during this interim period?

Page I-4 Minor Projects 2 – The exit ramp drafted here continues to apply the interim zoning control standards to projects that are determined to be minor. An alternative could apply the existing zoning standards to the review of minor projects.

**DECISION POINT:** Should minor projects be reviewed according the same goals, framework and standards as major projects?

Page DF-1 Zone 3 - The inclusion of the Rock Creek Drive Corridor in the interim zoning controls would allow greater flexibility of use and form in that area. Changes resulting from that flexibility could be seen as conflicting with the usage and form of other, relatively recent construction within that corridor (verbal examples will be provided).

**DECISION POINT:** Is the City Council ready to see changes to the Rock Creek Drive Corridor that could cause it to look similar to the development types promoted for the downtown area?

Page DF-1 Single-Family Residential – This page begins to incorporate the 3<sup>rd</sup>-way between allowing single-family homes outright and prohibiting them entirely and references "adaptive reuse" as a way to plan for the future while the present use is allowed.

**DECISION POINT:** See below.

Page DF-5 Build-To Line – This preference within the Development Framework is an instance where the standards related to form are stricter than the current standard. The stricter standards would only apply within Zone 1.

**DECISION POINT:** Does the Council accept the tightening of the form-based standards in Zone 1?

Page DI-2 Development/Amenities Desired – This page lists 8 preferred development scenarios for which qualified projects could obtain incentives. Five are related to public (primarily pedestrian) amenities. Two are related to

residential mixed use projects. One is related to other mixed use projects.

**DECISION POINT:** Do these development types and amenities reflect the goals of the Council?

Page DI-3 Incentives Available – This details 4 incentives that the City is prepared to offer and lists 2 others for consideration. Since parking requirements have been described as the single biggest burden to downtown development, 2 of the incentives would allow for waivers of the parking requirements. One incentive would allow greater use flexibility for mixed use construction. One (primarily applicable within Zone 3) would allow form flexibility.

**DECISION POINT:** Are these incentives adequate to induce the development the City desires?

Page PA-10 –A fundamental change to current practice, this page identifies ways where wider sidewalks can be developed. The program promoted would 1) require exactions during the development process and 2) typically qualify projects for development incentives when wider sidewalks are provided.

**DECISION POINT:** Does the provision of wider sidewalks justify the program described on this page?

Page PD-1 –The word “orientation” has been added to the existing text to indicate that new buildings should be parallel to the street like other buildings in the plan area.

**DECISION POINT:** Does the City Council wish to ensure building walls are parallel to the street?

Page PD-3 –The drafted changes encourage taller buildings than 1) typically exist downtown and 2) were promoted in the 1999 draft. The common cornice line or horizontal element is included as an attempt ensure these buildings are “good neighbors” to existing development.

**DECISION POINT:** Does the encouragement of taller buildings support the Council’s goals for development downtown?

Page PD-9 Sign Guidelines –The draft attempts to reconcile the 1999 proposal with the 2011 update to the City’s sign standards. Many of the suggestions promoted through the earlier document were incorporated in the later and shifting the policy stance 3 times in 9 years is likely to cause more confusion than the benefits derived from it.

**DECISION POINT:** Does Council wish to continue relying on the 2011 sign standards?

Page DS-5a through 5c Matrix Generally –This matrix moves the needle on several known issues in the plan area. For all zones it 1) promotes the idea upper story and lower story uses would differ (mixed use), 2) applies what are typically lesser parking requirements for development, and 3) attempts to dovetail with—not replace—the use provisions of our existing Zoning Code.

**DECISION POINT:** Does Council agree with the direction set though this matrix?

Page DS-5a through 5c Zone 1 –For Zone 1, the matrix 1) addresses a question of whether parking would be required of the Courthouse Plaza park project and 2) no longer allows construction of several currently allowed uses (including single-family residential but also additional service uses such as banks, salons, etc.).

**DECISION POINT:** Do the Zone 1 use and form requirements lead to the Council’s downtown goals?

Page DS-5a through 5c Zone 2 –For Zone 2, the matrix 1) allows the uses allowed in Zone 1, 2) rethinks how parking lots can be incorporated into the downtown area, 3) sets stricter form requirements for proposed single-family homes, which would be reviewed as conditional uses and 4) allows townhome development.

**DECISION POINT:** Do the Zone 2 use and form requirements lead to the Council’s downtown goals?

Page DS-5a through 5c Zone 3 –In Zone 3, the matrix provides a way for the City to be more flexible for proposals along the Rock Creek Drive corridor. The flexibility applies to both the use and the form of proposals which incorporate the development/amenities for which incentives are available.

**DECISION POINT:** Does the added flexibility in Zone 3 contribute to the Council’s goals for that area?

Page DS-5a through 5c Note 10 Multi-Family Development –The existing Zoning Code includes “apartments, cluster-type housing, condominiums, and multiple dwellings or groups of structures on a single lot” within its description of multi-family dwellings. The proposed change would remove the last part of the description to ensure that new single-family dwellings are not allowed simply because they exist on the same lot as another single-family dwelling. This change would impact a prospective proposal.

**DECISION POINT:** Does the Council want to steer multi-family residential construction toward apartments and mixed use buildings and away from groups of single-family buildings?

Page DS-5a through 5c Note 11 Single-Family Development –The draft note describes what the City would require to ensure a project is designed for future “adaptive reuse”.

**DECISION POINT:** Does single-family development, constructed to be easily converted to more economically productive land uses support the Council’s vision and goals for downtown?

### **Next Steps**

After the Council is confident that all relevant decision points above, and any decision points resulting from the Council’s discussion are addressed, staff will incorporate any requested changes into the document that may be used to regulatory effect as an interim zoning control. A public hearing will be scheduled to adopt findings of fact in support of the interim controls, and staff will focus efforts on development of the downtown plan. The Council should expect a contract proposal from downtown planning consultants at the March regular meeting.

Prepared by,

Ben Shumaker  
Community Development Director

**CITY OF STEVENSON**  
**ORDINANCE 2019-1138**

**ADOPTING INTERIM ZONING CONTROLS FOR THE  
DOWNTOWN AREA AND KEY COMMERCIAL  
CORRIDORS WITHIN THE CITY OF STEVENSON,  
REPLACING EXISTING DEVELOPMENT MORATORIA  
AND REPEALING ORDINANCES 2018-1127, 2018-1130,  
AND 2019-1139.**

**WHEREAS**, the City of Stevenson currently has four active development moratoria in effect:

- Ordinance 2018-1127, adopted on November 15<sup>th</sup>, 2018 prevents the siting of shipping containers within any Trade District (SMC Ch. 17.25) or within the Rock Cove Corridor (SMC Ch. 17.23). This ordinance limited effective area of and extended a moratorium adopted through Ordinance 2018-1124;
- Ordinance 2018-1128, adopted on November 15<sup>th</sup>, 2018 prevents new commercial connections to the City sewer system that will impact the BOD5 or TSS load on the wastewater treatment plant;
- Ordinance 2018-1130, adopted on December 6<sup>th</sup>, 2018 prevents new construction of single-family residences within the C1 Commercial District;
- Ordinance 2019-1139, adopted on February 21<sup>st</sup>, 2019 expanding the moratorium adopted through Ordinance 2018-1130 to include similar like uses; and

**WHEREAS**, a significant cause of the development moratoria involves the 1994 Zoning Code amendments' failed alignment with market realities of land development in Stevenson; and

**WHEREAS**, Ordinance 2018-1127 included a work plan for development of a downtown plan through which better alignment can be achieved between the City's zoning regulations and the land development market; and

**WHEREAS**, the City is currently reviewing proposals from consultants to develop such a plan; and

**WHEREAS**, instead of reactionary planning through moratoria, the City seeks to take a more facilitative but more directive approach while the downtown plan is being developed; and

**WHEREAS**, RCW 35A.63.220 provides that, "A council or board that adopts a moratorium or interim zoning control, without holding a public hearing on the proposed moratorium or interim zoning control, shall hold a public hearing on the adopted moratorium or interim zoning control within at least sixty days of its adoption, whether or not the council or board received a recommendation on the matter from the commission. If the council or board does not adopt findings of fact justifying its action before this hearing, then the council or board shall do so immediately after this public hearing. A moratorium or interim zoning control adopted under this section may be effective for not longer than six months, but may be effective for up to one year if a work plan is developed for related studies providing for such a longer period. A moratorium or

interim zoning control may be renewed for one or more six-month periods if a subsequent public hearing is held and findings of fact are made prior to each renewal"; and

**WHEREAS**, the provisions included in the interim zoning controls allow the city to process development applications and gauge the effectiveness of new regulatory provisions that may be included in the Zoning Code after the downtown plan is developed; and

**WHEREAS**, the adoption of interim zoning controls will allow the city to lift three of the four active development moratoria; and

**WHEREAS**, a work plan has been developed for studies related to the development of zoning standards and that work plan is attached to this Ordinance as Exhibit "B"; and

**WHEREAS**, RCW 35A.63.220 authorizes the enactment of interim zoning controls without holding a public hearing; and

**WHEREAS**, the Stevenson City Council shall hold a duly advertised public hearing on March 21<sup>st</sup>, 2019, to consider public comment concerning the authorization of such interim zoning controls; and

**WHEREAS**, the Stevenson City Council finds that the best interests of the City would be served if such a moratorium was authorized.

**NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF STEVENSON, STATE OF WASHINGTON DO ORDAIN AS FOLLOWS:**

**Section 1** – The *Downtown Stevenson Draft Development Framework, Standards & Design Guidelines* dated August 1999 and including *2019 Interim Zoning Control Annotations*, attached as Exhibit "B" is officially adopted as an interim regulatory supplement to the Zoning Code, Stevenson Municipal Code Title 17 – Zoning.

**Section 2** – The interim zoning controls adopted herein shall be effective for a one (1) year period as the City completes a downtown and commercial area planning process. The one (1)-year period is necessary based on the Work Plan established in Exhibit "A".

**Section 3** – The interim zoning controls adopted herein may be extended for one or more six (6) month periods in the event a work plan or further study determines that such extension is necessary or to determine what further policies and procedures related to downtown and commercial area development will best serve the purposes of the City.

**Section 4** – Ordinances 2018-1127, 2018-1130, and 2019-1139 together with the moratoria they impose shall be repealed in their entirety.

**Section 5** – If any section, subsection, sentence, clause, phrase, or other portion of this Ordinance, or its application to any person, is, for any reason, declared invalid, in

whole or in part by any court or agency of competent jurisdiction, said decision shall not affect the validity of the remaining portions hereof.

Passed by a vote of \_\_\_\_\_ at the special City Council meeting of March 5<sup>th</sup>, 2019.

SIGNED:

ATTEST:

\_\_\_\_\_  
Scott Anderson  
Mayor of Stevenson

\_\_\_\_\_  
Leana Kinley  
Clerk/Treasurer

APPROVED AS TO FORM:

\_\_\_\_\_  
Kenneth B. Woodrich  
City Attorney



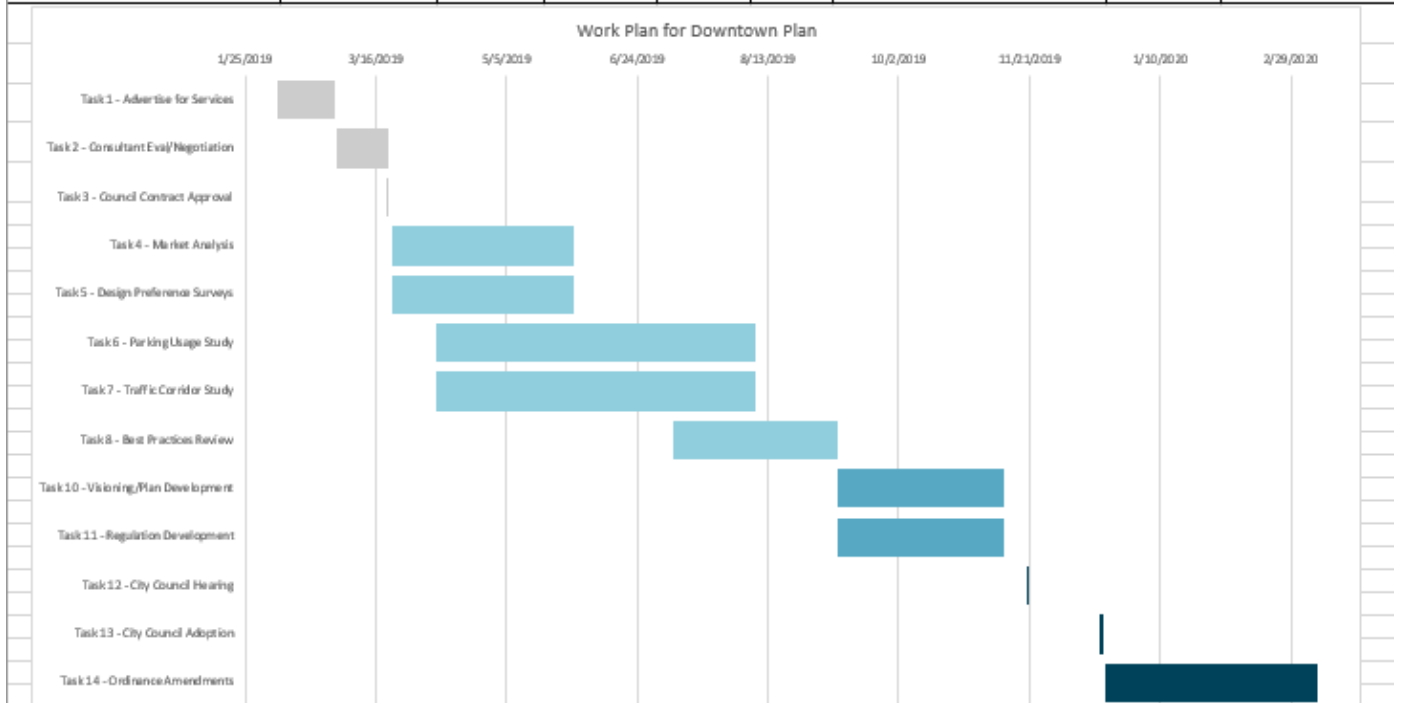
**Ordinance 2018-1138  
Exhibit "A" Work Plan**

**Downtown Plan**

City of Stevenson

Planning Department

TASK NAME	ASSIGNED TO	START DATE	DUE DATE	DURATION	% DONE	DESCRIPTION	PRIORITY	SPRINT/MILESTONE
Task 1 - Advertise for Services		2/6/2019	2/28/2019	22				Phase 1
Task 2 - Consultant Eval/Negotiation		3/1/2019	3/21/2019	20				Phase 1
Task 3 - Council Contract Approval		3/20/2019	3/21/2019	1				Phase 1
Task 4 - Market Analysis		3/22/2019	5/31/2019	70				Phase 2
Task 5 - Design Preference Surveys		3/22/2019	5/31/2019	70				Phase 2
Task 6 - Parking Usage Study		4/8/2019	8/8/2019	122				Phase 2
Task 7 - Traffic Corridor Study		4/8/2019	8/8/2019	122				Phase 2
Task 8 - Best Practices Review		7/8/2019	9/9/2019	63				Phase 2
Task 10 - Visioning/Plan Development		9/9/2019	11/11/2019	63				Phase 3
Task 11 - Regulation Development		9/9/2019	11/11/2019	63				Phase 3
Task 12 - City Council Hearing		11/20/2019	11/21/2019	1				Phase 4
Task 13 - City Council Adoption		12/18/2019	12/19/2019	1				
Task 14 - Ordinance Amendments		12/20/2019	3/18/2020	89				Phase 4
		12/20/2019	3/18/2020					



**Ordinance 2018-1138**

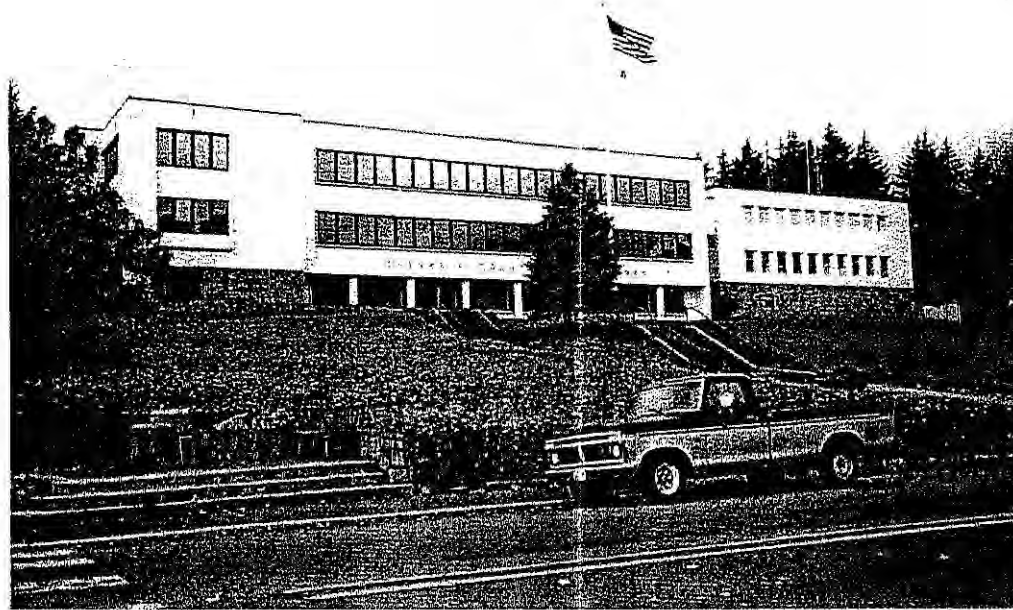
**Exhibit "B"**

**"Downtown Stevenson**

**Draft Development Framework, Standards & Guidelines"**

**"2019 Interim Zoning Control Annotations"**

# DOWNTOWN STEVENSON



## DRAFT DEVELOPMENT FRAMEWORK, STANDARDS & DESIGN GUIDELINES

August 1999

CITY OF STEVENSON, WASHINGTON

CRANDALL ARAMBULA PC

## 2019 Interim Zoning Control Annotations

These annotations are prepared to support use of the 1999 *Downtown Stevenson Draft Development Framework, Standards & Design Guidelines* as an informative background document and supplement to the Zoning Code. The supplement will have regulatory effect as interim zoning controls.

This 1999 draft was selected because it is the most recent comprehensive look at downtown development. While its age has reduced some of its relevance, the concepts promoted within it still largely align with the direction currently being discussed by many in the community. This 1999 proposal is seen as more relevant than the 1991 study *Planning for Downtown Revitalization, Stevenson, Washington* which served as the basis of Stevenson's existing zoning standards, a set of standards that is now seen as ineffective and contrary to the improvement of vitality downtown.

Where it has been deemed necessary to change the 1999 document, those changes are highlighted in **purple** and the change is explained in this column, at the bottom of pages containing maps, or as wholly new pages within the document. Where changes or information is added to this document based on other downtown planning efforts, they are highlighted in **blue**.

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# 2019 Interim Zoning Control Annotations

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## **Introduction**

Intent

Downtown Development Goals & How to Use  
Submittal Requirements

Design Review Process & Major/Minor Projects



## Intent

Stevenson's downtown is the center of Skamania County's retail, commercial, and governmental activities and continues to grow as a destination for tourists and recreationalists. Over the last few years, concerted efforts have been initiated by local officials, community leaders and business owners to increase the vitality and viability of downtown businesses.

The following Development Framework, Standards and Design Guidelines ensure that new construction or renovation of existing buildings is:

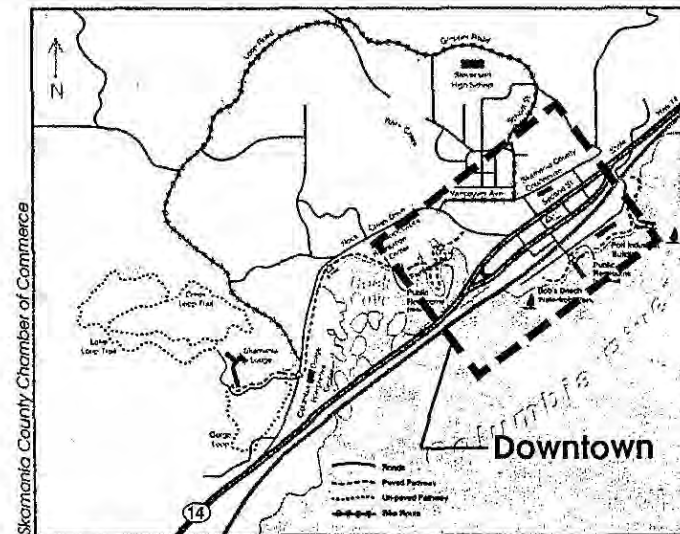
- Consistent with the historic character of Downtown Stevenson.
- Of high quality and long lasting materials.

The Development Framework, Standards and Design Guidelines will:

- Assure that each development site is consistent with Stevenson's Framework Plan and existing zoning.
- Assure specific descriptive requirements and guidelines for streetscape improvements and development site planning, architecture, lighting, and signs are addressed.
- Ensure the urban character of downtown is maintained without dictating or prescribing a specific architectural style.
- Be easy to use by both developers and reviewing government agencies.



Second Street - The retail artery of downtown



Vicinity Plan

## 2019 Interim Zoning Control Annotations

- Simultaneously consistent with the historic character of Downtown Stevenson while also acting as a bridge to Stevenson's prosperous future.



## Downtown Development Goals

- Create a pedestrian-friendly downtown.
- Increase commercial and retail development.
- Increase residential development.
- Use existing downtown infrastructure, open space and buildings.
- Integrate and enhance the value of adjacent natural features such as the Columbia River and views across and along the Gorge.

## How to Use

The Development Framework, Standards and Design Guidelines apply to new construction and major renovations or replacement of existing lighting or signs within the downtown plan area. This area extends from the intersections of First and Second Streets to the east and west, and from Vancouver Avenue to the Columbia River.

### Development Framework

The Development Framework serves as a basis for the Development Standards and Design Guidelines. It establishes a land use and circulation framework for future development and rehabilitation or re-use of properties within downtown Stevenson.

### Development Standards

The Development Standards supercede the existing Development Code where discrepancies exist. They are regulatory requirements which are not negotiable during the Design Review Process.

### Design Guidelines

Design Guidelines support the Development Framework and Development Standards. They apply to all development that occurs downtown, and are administered through the design review process. The Commission may waive individual guidelines for specific projects based on findings that the waiver will better accomplish the goals. Should the Stevenson Planning Council find that one or more design guidelines are not applicable to the circumstances of the particular project being reviewed, the guideline(s) may be waived by the Stevenson Planning Council.

The Stevenson Planning Council may also address aspects of a project's design which are not covered in the guidelines where the Planning Council finds that such action is necessary to better achieve Stevenson's downtown development goals.

## 2019 Interim Zoning Control Annotations

- Create inviting connections between downtown and the Skamania Lodge.

This area is depicted as Zones 1 through 3 on Page DF-2.

The Development Framework serves as the basis for the Development Standards, Design Guidelines and Development Incentives.

They are regulatory requirements which are not negotiable during the Design Review Process. Projects must fit into the box established by the Development Standards.

Design Guidelines support the Development Framework and Development standards. All projects are encouraged to follow the Design Guidelines, but proponents are also free to "color outside the lines". The Planning Commission has wide latitude to waive individual guidelines and may do so for projects based on findings that:

- The waiver will better accomplish the Downtown Development Goals, or
- One or more design guidelines are not applicable to the circumstances of the particular project being reviewed.

SEE NEW PAGE I-2a.

2019 Interim Zoning Control Annotations

How to Use, Continued

Development Incentives

Development Incentives provide for reduced regulatory burdens, reduced site planning constraints, or other benefits to projects. Development Incentives are intended to encourage projects to “color inside the lines” when designing the project. Proposals involving Development Incentives are reviewed by the Planning Commission during the design review process, and the decision to provide the Development Incentive is made on a purely discretionary basis.



Submittal Requirements

Applications for design review may be obtained from the City of Stevenson. An application for review by the Stevenson Planning Council must be accompanied by a set of drawings including the following:

New Construction Projects

- A site plan including the shape and location of all buildings and major land uses within 200’ of the project site perimeter.
- A complete set of building elevations, isometrics and/or perspectives. (A list of materials and color to be used must be indicated on drawings. Samples may be submitted.)
- A parking plan showing the location of all on-site parking and loading facilities, including access and egress routes.
- A ground floor plan showing all grade changes and indicating the use of various spaces.
- Typical floor plans as necessary to adequately describe all levels of the building.
- Sections of the project or building to adequately describe the shapes and relationships of spaces.
- A landscape plan showing all planting areas, street furniture, street trees, outdoor art, and other outdoor features of the project, including a list of materials and colors.

- Sign plans showing all sizes, materials and locations on the building or site
- Lighting plan showing all types, materials and locations on the building or site
- Identification on site plan of location and species for all trees with a 5” or greater caliper measurement. Trees to remain as well as trees designated for removal are to be included. Historical significance of trees and project impact will be reviewed by City Staff. Additional assessment of impacted significant trees by an arborist may be required and mitigation measures to be included in Design Review application may be required.

Remodeling or Rehabilitation Projects

- All of the new construction requirements that are relevant as determined by city staff

•Projects are encouraged to provide a master signage plan for review and approval at the time a major project is initiated. Approved master signage plans may then be provided to tenants and the City to streamline review of future signage proposals.

—

•...Additional assessment of impacted significant trees (as that term is defined in SMC 18.13.010(B)(48)) by an arborist may be required and mitigation measures to be included in Design Review may be required.



## Design Review Process

### Criteria

Design Review does not require that every decision maker become skilled in design and design analysis. It does demand that the people involved in important design decisions recognize and enforce the strong organizing patterns that currently exist. The Design Guidelines let decision-makers know what to look for and provides them the criteria to objectively consider and evaluate project proposals.

Compliance with the Design Guidelines can take many forms for a given project. The guidelines are meant to state broad design objectives and shall not be construed as prescriptive standards.

### Preapplication Meeting

Project designers and developers are strongly encouraged to request an early briefing with the Stevenson Planning Council and/or City Staff prior to formal application for a hearing. Such meetings provide an opportunity for informal discussion of the specific circumstances of the project and how the Development Framework, Standards and Design Guidelines might effect its development.

## Major and Minor Projects

Proposals reviewed by the Stevenson Planning Council will be classified as major or minor projects.

**Minor Projects:** A minor project is one that the Planning Director and the Planning Council Chairman find will not significantly affect the character, use, and development of the surrounding properties. Minor projects will be reviewed for consistency with the Development Framework, Standards and Design Guidelines by the Planning Director and staff.

**Major Projects:** Major projects as determined by chairman will be reviewed by the Planning Council for consistency with applicable design guidelines.

Upon receipt of an acceptable application adequately describing the project, a hearing on the proposed project will be scheduled. The application must be received at least 10 days before the next regularly scheduled Planning Council Meeting. Upon receipt of an acceptable application adequately describing the project, a hearing on the proposed project will be scheduled at the next Planning Council Meeting.

Should an applicant object to any aspect of a decision by the Design Commission or City Staff, they have 20 days in which to file an appeal with the Stevenson Boundary Review Board and subsequently with Superior Court.

## 2019 Interim Zoning Control Annotations

During the effective period of the 2019 Interim Zoning Controls, all projects are considered major projects. In the discretion of the Zoning Administrator, after consultation with the Design Liaison Committee, the Zoning Administrator may consider a project as minor. Minor projects will be reviewed for consistency with the Interim Zoning Controls by the Zoning Administrator.

Project designers and developers are strongly encouraged to request an early briefing with the City prior to formal application for design review by the Planning Commission. Such meetings provide an opportunity for informal discussion of the specific circumstances of the project and how the Development Framework, Standards, Incentives and Design Guidelines might effect its development. For the purposes of implementing these interim zoning controls, an ad hoc Design Liaison Committee will be formed consisting of the Zoning Administrator, a City Council Member, and a member of the Stevenson Downtown Association.

Should an applicant object to any aspect of a decision by the Planning Commission or City Staff, they have 20 days in which to file an appeal with the Stevenson Board of Adjustment and subsequently with Superior Court.

*No annotations to this page.*

## **Development Framework**

Framework Concept & Development Framework Features

Development Framework Diagram

Circulation Framework Features

Circulation Framework Diagram

Build-To Line

Active Street Frontage

Rain Protection

Build-To Line, Active Street Frontage & Rain

Protection Diagram



Framework Concept & Development  
Framework Features

To foster an economically viable downtown, enhance Stevenson’s character and preserve its heritage for future generations, the following pages describe a land use and circulation framework for future development and rehabilitation or re-use of properties within Downtown Stevenson. Often land use and transportation improvements are made without an overall plan or recognition of the relationship to adjacent neighbors. This can lead to conflicts between competing uses.

Compatible Uses

The Framework Plan for Downtown Stevenson prescribes a “game plan” so that future land use, transportation and open space improvements are built in a manner that is respectful and complementary to its context, thereby creating an understandable, distinctive and memorable downtown.

Confidence and Certainty

The Downtown Stevenson Framework Plan establishes the environment in which developers and existing business people can be assured that Stevenson is a good place to do business. “Zone 1” is the primary retail zone and “Zone 2” is the supporting commercial zone. The framework:

- Maximizes current investment in buildings and infrastructure.
- Supports and strengthens Second Street as the primary retail armature (Zone 1).
- Suggests that retail uses along Russell Street should be fostered to tie the Columbia River into downtown.
- Recognizes that there is a limited marketplace. The framework suggests that First Street be a secondary “mixed use” commercial street that does not compete with Second Street, but rather complements it with supporting services.
- Suggests that waterfront business along Cascade Street should foster and support tourism, industry and recreational uses.
- Suggests that civic uses should be limited to Vancouver and Russell Streets (north of Second Street).
- Promotes additional residential uses, especially townhouse and apartment development in the downtown area.
- Promotes the development of civic spaces and attractors - i.e. Courthouse Square.

...“Zone 1” remains the primary retail zone, “Zone 2” is the supporting commercial zone that allows expansion of the primary retail zone, and “Zone 3” is a transitional area that allows greater flexibility of use while providing building forms complementary to the development framework and which facilitate adaptive reuse in the future. The framework:...

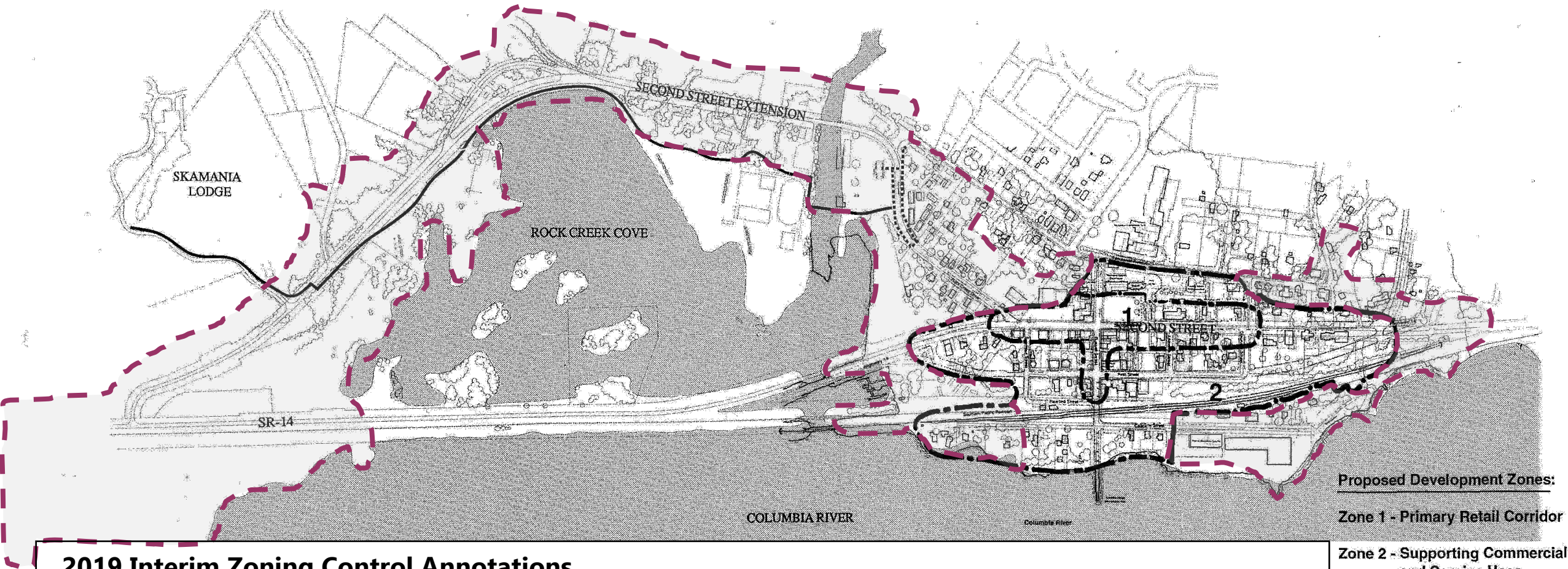
—  
•...Recognizes that there is a limited marketplace. The framework suggest that First Street and Rock Creek Drive be secondary “mixed use” commercial corridors that do not compete with Second Street, but rather complement it with supporting residences and services...

—  
•Suggests that civic uses should be limited to Vancouver Avenue, Russell Street (north of Second Street), and Rock Creek Drive.

•Promotes additional residential uses, especially townhouse, apartment, and mixed use development. Single-family residences are discouraged in downtown area unless designed and constructed to support adaptive reuse.



Development Framework Diagram



Proposed Development Zones:

Zone 1 - Primary Retail Corridor

Zone 2 - Supporting Commercial and Service Uses

Zone 3 - Transitional Area

2019 Interim Zoning Control Annotations

The boundaries between zones 1 and 2 remain as they were proposed in 1999, however many Zone 2 areas—especially areas extending eastward along 1st and 2nd streets—are now seen as a part of the primary retail corridor. This current direction is not prohibited by the 1999 proposal which allows properties in Zone 2 to develop as if they were within Zone 1, while also allowing greater flexibility in how the properties may be used. Zone 3 is added to the development framework in purple. Properties within Zone 3 only qualify for the framework’s regulatory flexibility if they are served by the public sewer system, which facilitates the building density anticipated. The area along Cascade Avenue zoned R3 Multi-Family Residential has been excluded from the previously proposed Zone 2 and included within Zone 3.

*No annotations to this page.*

## Circulation Framework Features

### The Pedestrian is the Priority

The pedestrian must have the highest priority in the network of roadways, pathways and open spaces that cross and connect downtown. This network can be simply described as an environment where a humane, walkable cityscape has been preserved or created without compromises. However, this does not preclude the importance of providing motor vehicle access to maintain downtown as the center of commerce and civic life.

The public investment in infrastructure serves as a catalyst for change and an impetus for private sector investment. The Framework Plan provides a blueprint for the implementation of positive changes in the appearance, livability and safety for residents, downtown employees and visitors. The framework:

- Bridges all existing pedestrian obstacles and provides a clear and understandable connection to the Columbia River.
- Provides pleasurable areas for strolling, resting, watching, eating and socializing.
- Considers and incorporates elements that respond to the environment, especially the winter season.
- Is barrier-free and accessible to people of all ages and physical conditions.
- Slows truck and auto traffic downtown by incorporating “traffic calming” devices such as curb extensions.



[illegible]

The lines added in **purple** indicate pedestrian projects that have been considered more recently. Cross sections for those projects are not included herein.

No annotations to this page.

Build-To Line

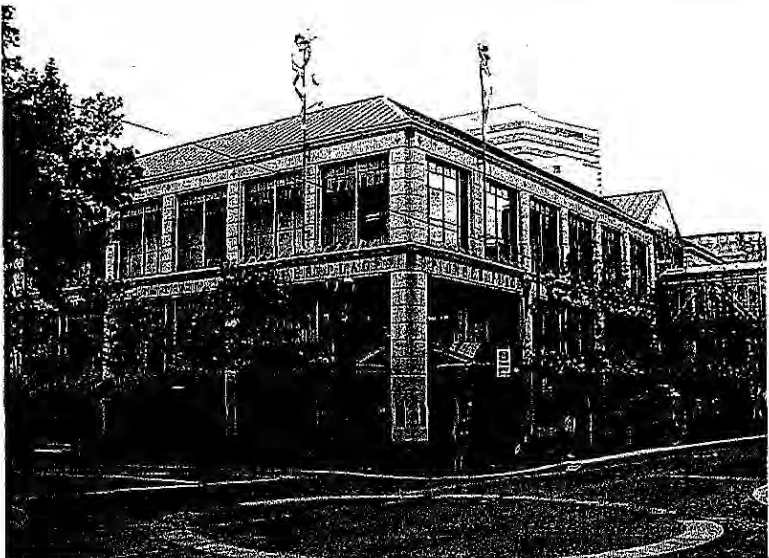
Buildings along designated streets must be built out to the property line with no setbacks allowed to ensure design consistency with historic structures, and to promote a sense of urban enclosure. This contrasts to suburban environments where buildings do not engage the street causing gaps and discontinuities, eroding the retail environment at the street line and destroying any sense of urban enclosure.

Required:

- 0' front yard setback (see yard requirements, Development Standards Matrix, Page DS-3)
- Recessed doorways set back (5' maximum), especially at corners
- Recessed windows (8" maximum), especially at groundfloor

Prohibited:

- Plazas or landscaped forecourts
- Auto/Service drop-offs or pullouts



**Appropriate:** Retail building with no setback, open and engaging storefronts, and awnings



**Not Appropriate:** Retail building set back to provide angled parking in front erodes street life and an interesting visual environment for pedestrians



*No annotations to this page.*

## Active Street Frontage

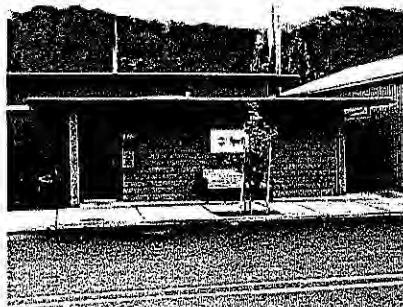
The design of the ground floor of buildings along designated active streets should be open and inviting, engage the pedestrian, and help create activity along the sidewalk and street. Wide openings between buildings, parking lots, and impenetrable blank building walls and lack of windows and entries discourage pedestrian use of the sidewalk and compromise the overall activity of the street. Active streets and sidewalks are created when buildings are continuous, inviting, and employ quality materials and detailing, especially at the ground floor.

### Required:

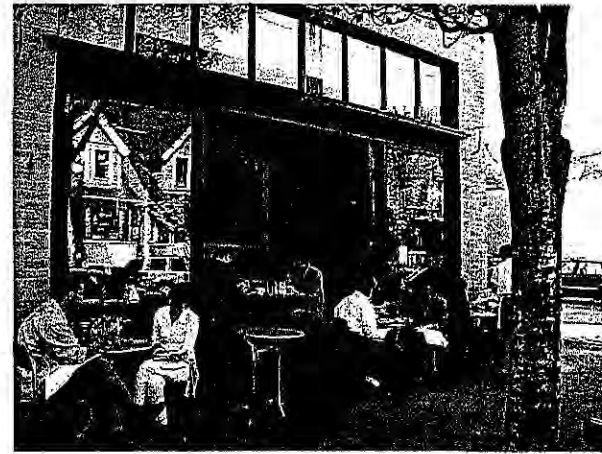
- Primary building entry along street
- 50% (minimum) glazing at groundfloor street frontages
- Storefront windows

### Prohibited:

- Blank walls
- Tinted, reflective or opaque groundfloor glazing



**Not Appropriate:** "Dead" walls with no



**Appropriate:** Large cafe windows and outdoor seating



**Appropriate:** Large retail windows and rain protection

# Rain Protection

## Awnings & Canopies

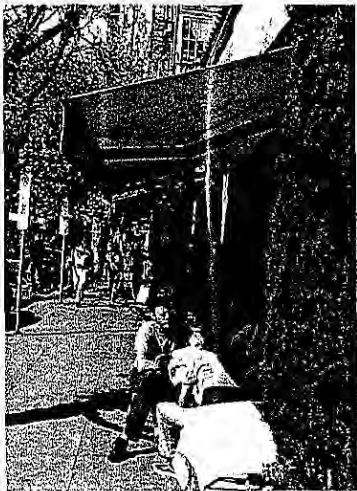
Awnings and canopies will encourage pedestrians to comfortably use sidewalks during inclement weather. They should be designed as an integral component of the building facade.

### Required:

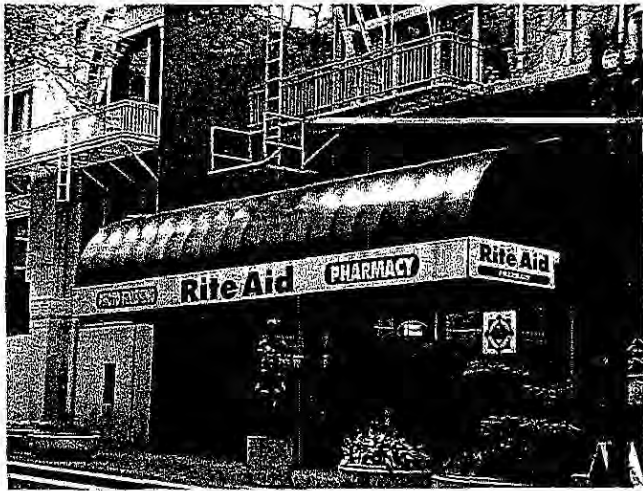
- Canvas fixed or retractable awnings
- Horizontal metal canopies

### Prohibited:

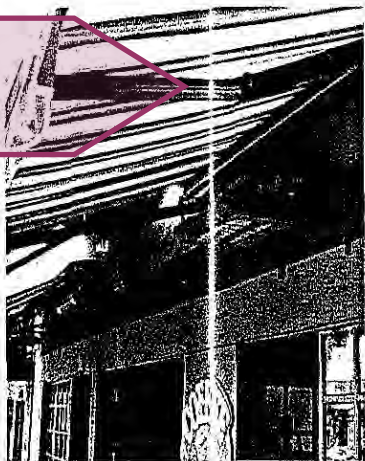
- Vinyl or other synthetic fabrics
- Backlit awnings
- Oversized advertising or tenant signs on awnings



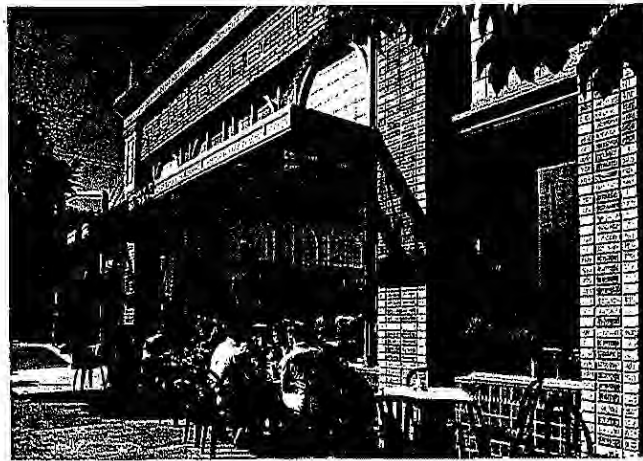
Appropriate: Retractable fabric awnings



Inappropriate: Vinyl awnings



Appropriate: Space heaters mounted below awnings

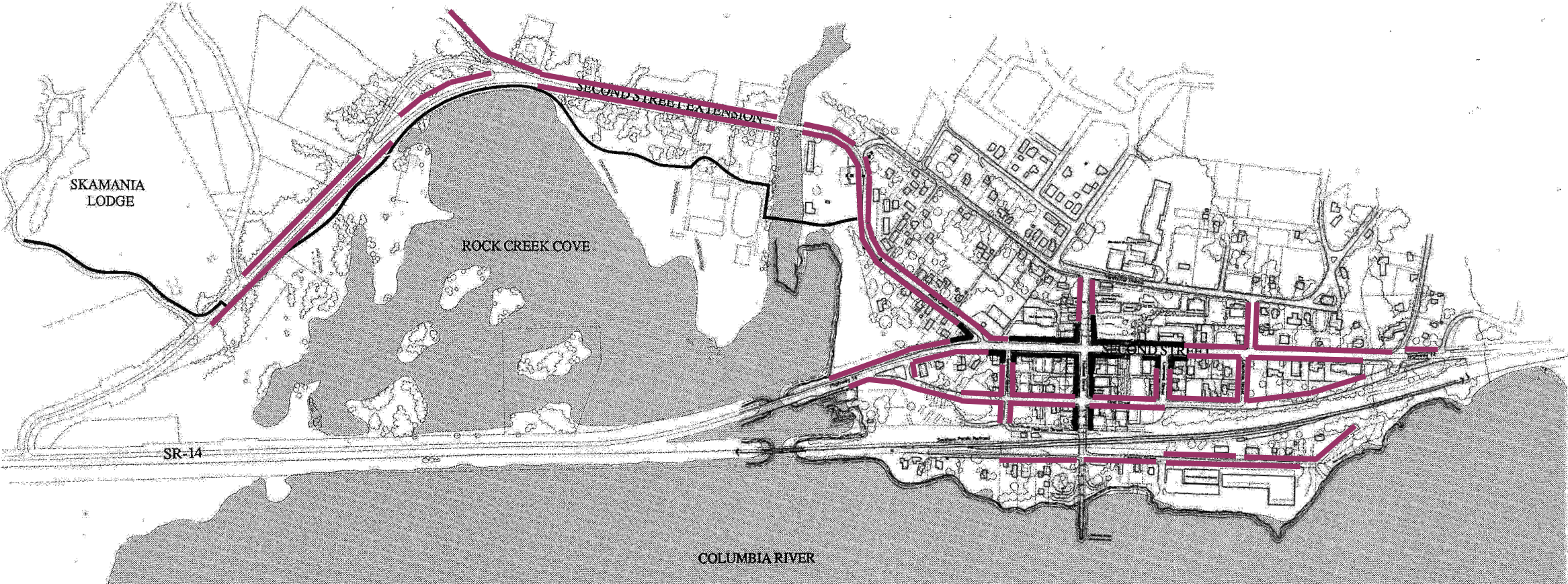


Appropriate: Canopies



- Oversized advertising or tenant signs on awnings [unless permitted through SMC 17.25.140.C.1](#)



Build-To Line, Active Street Frontage and Rain Protection



Columbia River

-  Build-To Line, Active Street Frontage and Rain Protection Required
-  Incentive Areas (Eligible when project provides Build-To Line, Active Street Frontage and/or Rain Protection)

# 2019 Interim Zoning Control Annotations

## Development Incentives

- Purpose
- Development/Amenities Desired
- Incentives Available
- Administration of Incentives

# 2019 Interim Zoning Control Annotations

## Purpose

Stevenson’s existing development framework was established through a major zoning change in 1994. At that time, the Skamania Lodge was just opening, and the development framework anticipated a strong market for new development and increased intensity of uses—particularly retail uses—in the downtown area.

That market has largely never materialized. A comparison of available aerial imagery from 1993 and 2018 shows that ~15 buildings have been demolished (not including the trailer park spaces removed as a result of the westward extension of 1st Street). In that same time period, only ~7 buildings (not including the public bath– and changing rooms along the waterfront) have been built. Further evidence of a weak market for commercial development is demonstrated in the fact that of the 7 new buildings downtown, 3 of them have been single-family dwellings. Two of the single-family dwellings constructed were located on Cascade Avenue and have forever changed the development character and potential usage of the Columbia River waterfront in that area.

These Development Incentives are a response to the weak market and intended to promote development of mixed use construction and to make the pedestrian the priority at Stevenson’s main commercial areas and corridors.



2019 Interim Zoning Control Annotations

- Construction of other public amenities listed herein.

Development/Amenities Desired

Recognizing the market limitations, Stevenson is not asking for a great deal in exchange for incentives. All properties in Zones 1, 2, and 3 are eligible for incentives provided any of the following are incorporated into the development:

- Mixed use buildings involving upper-story residential dwellings.
- Inclusion of dwelling units intended for occupancy as affordable housing (SMC 17.10.385) and/or workforce housing (SMC 17.10.387).
- Mixed use buildings involving upper story space available for rent to supporting business uses (e.g., office, child day care, overnight lodging, etc.)
- Construction incorporating active street frontage (Page DF-6) or rain protection (Page DF-7) features along frontages designated on the map on Page DF-8. (Required Areas & Incentive Areas)
- Construction of curb extensions and/or pedestrian street crossings at the locations identified on the maps on Pages PA-3 and PA-13.
- Provision of easement for and construction of sidewalks as identified on Page PA-10.
- Provision of easement for public pedestrian ways (SMC 17.10.660) contributing toward the City's goal for continuous pedestrian access along the Columbia River, Rock Cove and Rock Creek.



2019 Interim Zoning Control Annotations

within Shoreline Jurisdiction must also obtain approval under the City's shoreline master program.

Incentives Available

The following Development Incentives are available to projects:

Waiver of Parking Requirement—Ground Floor: The Planning Commission shall waive the parking required of ground floor uses when a project involves the addition of a mixed use building having upper-story residential dwellings.

Waiver of Parking Requirement—Upper Floor: The Planning Commission may waive up to 50% of the parking required for upper floor uses when a project a) includes affordable housing and/or workforce housing or b) provides an acceptable mix of the desired development/amenities listed on Page DI-2.

Use Flexibility: The Planning Commission may allow land uses not listed in the Development Standards Matrix when proposed as part of a mixed use building incorporating the uses listed on Page DI-2.

Dimensional Standards Waiver: Where required by the underlying zoning standards, required front yard setbacks or building height limitations\* may be waived by the Planning Commission when a project involves the pedestrian amenities listed on Page DI-2.

OTHER: \_\_\_\_\_TBD—FOR COUNCIL DISCUSSION\_\_\_\_\_

\_\_\_\_\_Systems Development Charge Waiver\_\_\_\_\_

\_\_\_\_\_Sewer Moratorium Waiver (Pretreatment)\_\_\_\_\_

\_\_\_\_\_Others?\_\_\_\_\_

\_\_\_\_\_TBD—FOR COUNCIL DISCUSSION\_\_\_\_\_.

\*Note: Proposals to exceed the 35' building height limitation imposed

2019 Interim Zoning Control Annotations

Administration of Incentives

The Planning Commission has the authority to grant development incentives during the Design Review process. The decision to provide a Development Incentive is made on a purely discretionary basis.

Applications for projects seeking development incentives shall clearly describe the incentive requested and how the project will provide the desired development/amenities.

The provision of a development incentive may be subject to such performance assurances as deemed necessary and as acceptable to the City Attorney.

## **Design Guidelines - Public Areas**

Purpose

Landscaping

Landscaping Diagram

Street Lighting

Street Lighting Diagram

Sidewalks

Sidewalks Diagram

Sidewalks Sections

Curb Extension

Pedestrian Crossing

Pedestrian Crossing Diagram

Street Furniture

Street Furniture Diagram

Railings and Fences

Retaining Walls

## Purpose

The streets and public spaces within Downtown Stevenson should not be considered simply as conduits for traffic to move through. Rather, they are enclosed “outdoor rooms” that provide for auto, bicycles, trucks and pedestrians alike. An environment should be fostered that encourages window shopping, strolling and sitting. To improve the downtown public area, the Design Guidelines:

- Preserve the riverfront as a public amenity and provide convenient, direct and safe access.
- Require new development that is more pedestrian friendly in character by promoting development that is compact and less auto dependent.
- Promote the use of unifying elements that strengthen downtown, by adding new elements that unify and connect development zones, individual facades and blocks.
- Enhance, embellish and identify the downtown as a unique place. Incorporation of unique, small scale features add to downtown’s ambiance. The elements must consider and reinforce Downtown Stevenson’s history and traditions.
- Establish and maintain a sense of urban enclosure. The right-of-way should contribute to the sense of human scale by enclosing the street edge with a continuous building wall built out to the property line. Such enclosures contribute to Stevenson’s pleasant, rich and diverse pedestrian experience. When the right-of-way rooms are not defined, the sense of urban enclosure is lost.
- Strengthen gateways. The guidelines promote gateways that provide a graceful transition into downtown, providing a sense of welcome and civic pride.
- Recognize and enhance the pedestrian system. Promotes the establishment of different zones of a sidewalk, curb, street furniture zone, walking and window shopping zone.
- Protect the pedestrian from vehicles, bicycles and the environment.
- Provide stopping and viewing places.
- Foster plazas, squares and open spaces that are inviting and enjoyable. Discourages overscaled, lifeless and difficult to maintain spaces.
- Integrate barrier-free design. Promotes design that is integral to the right-of-way rather than merely meeting minimum code standards.

## 2019 Interim Zoning Control Annotations

*No annotations to this page.*

*No annotations to this page.*

# Landscaping

## Intent

Landscaping should beautify streets and public spaces, promote pedestrian activity, unify the downtown by providing common elements, and establish places for public assembly.

### Appropriate:

- Rows of street trees that reinforce street enclosure, establish a rhythm and unify downtown with common elements.
- Shrubs and groundcover that soften and reduce the impact of paved surfaces.
- Plant materials selected to be dynamic and changing. Color during all seasons - fall, winter, spring, and summer.
- Long lasting and vigorous plants
- Minimal maintenance - Plant materials that require little irrigation, pruning and drop few leaves, seeds or fruit.
- Seasonal, annual planting beds, pots or baskets that add color and fragrance.
- Large stonework, particularly indigenous stones and boulders, integrated into landscaping.

### Inappropriate:

- Exotic materials
- Obstruction of sight lines at intersections. Branch height of canopy trees should be sufficient to avoid clipping by trucks.

2019 Interim Zoning  
Control Annotations

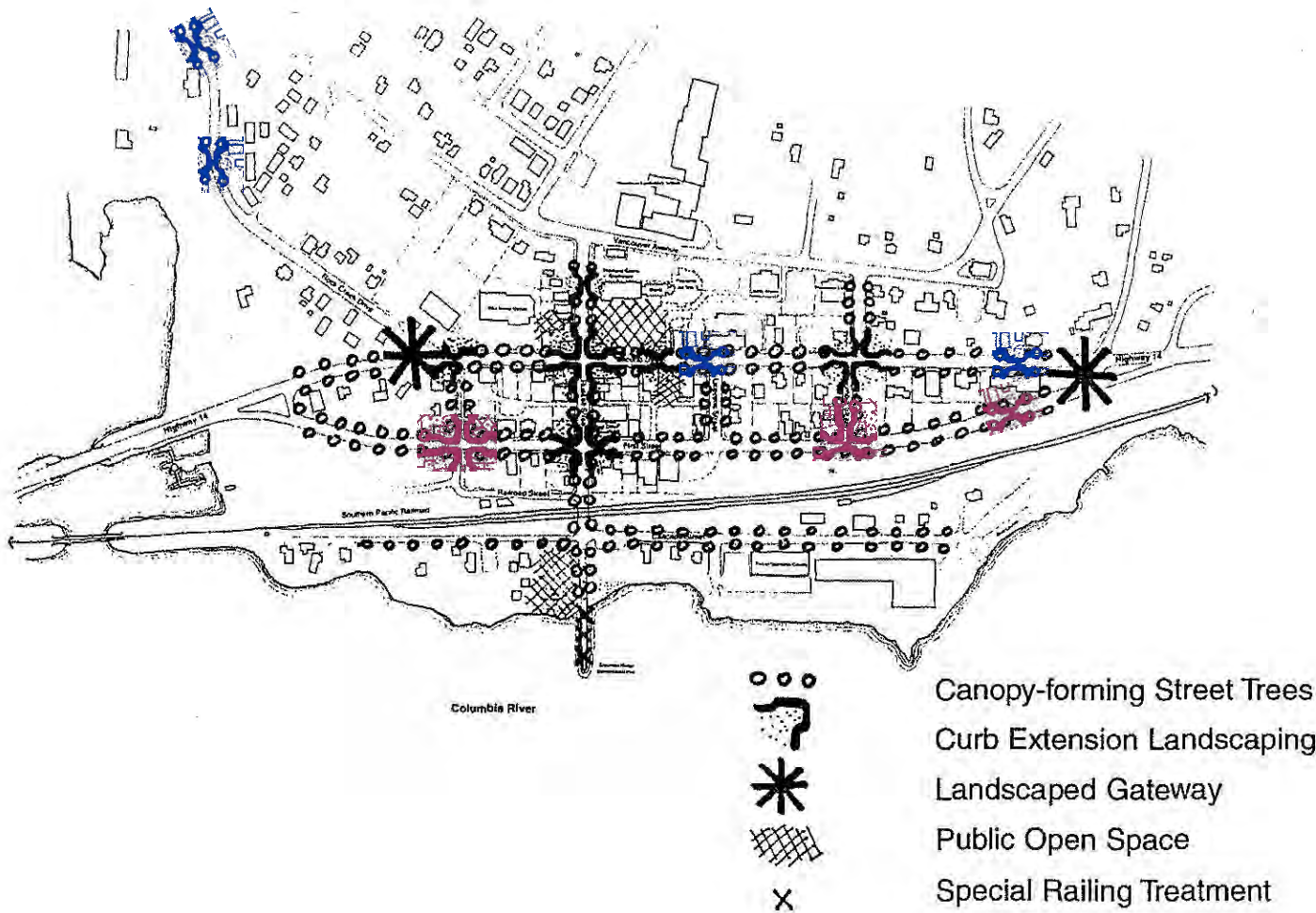
This map does not include the improvements made to or planned for Rock Creek Drive. New landscaping within public areas should be considered on a case-by-case basis.

The lines added in **blue purple** indicate curb extension landscaping that has been installed in addition to what was planned in 1999.

The lines added in **purple** indicate curb extension landscaping that has been planned in addition to what was planned in 1999.

- The City has not prioritized canopy-forming street trees in the projects that have been implemented since 1999. Such trees should be considered when new designs are formalized, but this map should not be treated as mandating them.

Landscaping





Street Lighting

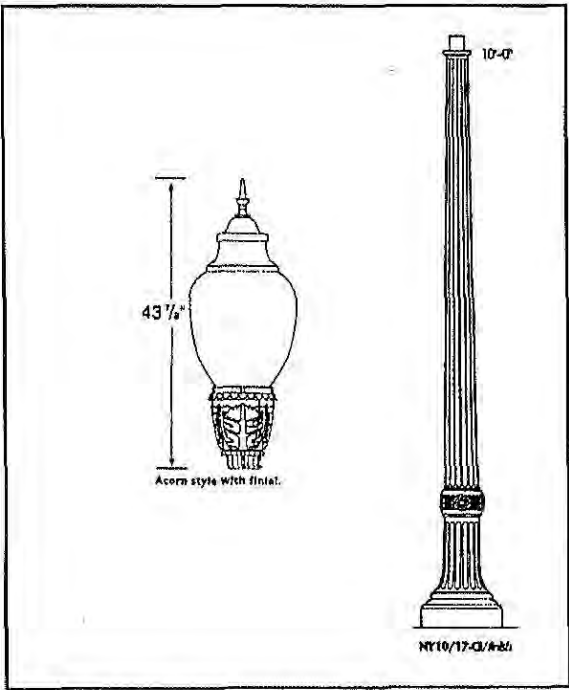
Intent

Street lighting should be designed to provide a safe environment for both motorists and pedestrians. Historic ornamental fixtures that are in character with downtown architecture and are compatible with the street furniture should be used.

Street lighting should provide warm “halo-like” glow that accents and provides interest and depth to adjacent buildings. It may be a fixture that uplights the branches and leaves of trees.

Street lighting should be pedestrian-scale (15' standards) and spaced at regular intervals (30').

Pole standards should be black or a very dark green color. Standards may accommodate banners and hanging flower pots. Automatic drip irrigation for the pots should be considered.



Appropriate: Historic style lamp and post, scaled to pedestrians.

For the specifications of the decorative lights used on 2nd Street and elsewhere, refer to the Holophane product described in “City of Stevenson SR14/2nd Street Couplet Upgrade, Street Illumination System”. Copies are stored with Stevenson Engineering Standards, Volume 3.

Consult the Public Works Department for information on light color/temperatures used during the 2018 LED conversion project.

Street Lighting

Appropriate:

- Historic street lights and parking lot lights
- Combined light fixtures, traffic signals and pedestrian signals
- Pedestrian-scaled height -15'

Inappropriate:

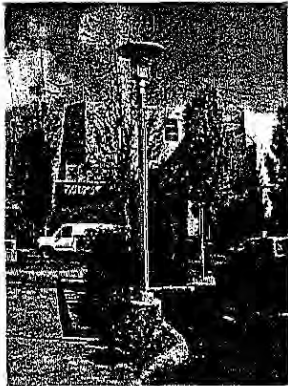
- Cobra-head type fixtures
- Contemporary fixtures



Appropriate: Pedestrian scaled street lights



Not Appropriate: Cobra head fixture



Not Appropriate: Contemporary light fixtures



Not Appropriate: Low, contemporary fixtures



Not Appropriate: Contemporary adaptations of traditional fixtures

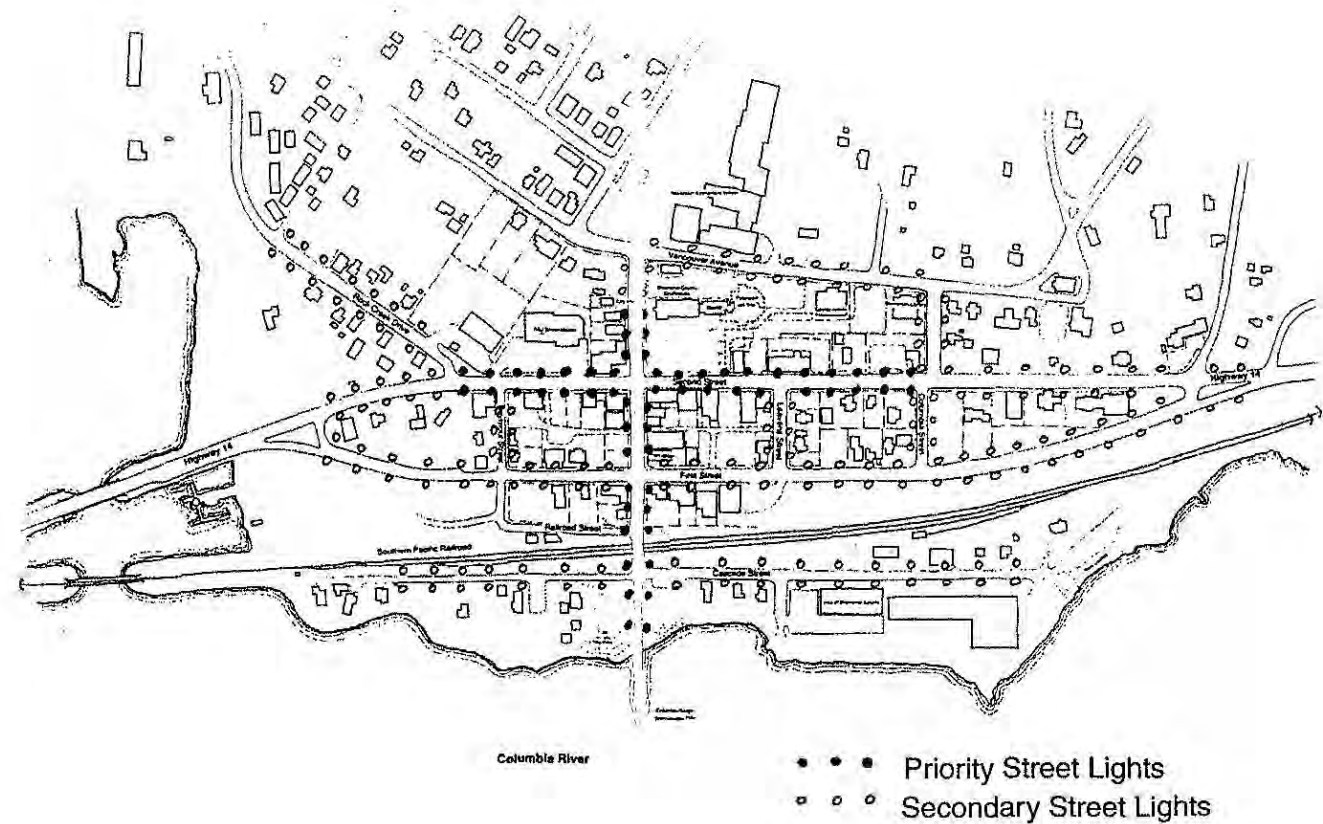
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Consult the Public Works Department for information on light color/temperatures used during the 2018 LED conversion project.

2019 Interim Zoning  
Control Annotations

This map does not include the improvements made to or planned for Rock Creek Drive. New lighting within all areas of Zones 1 through 3 should be considered on a case-by-case basis.

Street Lighting



Note: Secondary Street  
Lights are indicated at  
the pathway system.



No annotations to this page.

## Sidewalks

### Intent

The key to developing a successful Downtown Stevenson is to make the pedestrian environment the highest priority. Towns and cities that emphasize the speedy movement of automobiles at the expense of the pedestrian environment are usually lacking economically, environmentally and aesthetically. A positive experience for pedestrians enhances the economic vitality of the downtown and improves the quality-of-life for all.

A pleasant environment for pedestrians begins with the sidewalk. Essential components include a network of contiguous sidewalks that are wide enough for walking, street furniture, and trees. To grow properly, canopy streets trees need sidewalks that are at least 10' feet wide. When mature, trees break down the scale of the street, clean the air, provide shade, and improve the look and feel of the street. Grates provide the best base for street trees and discourage weeds and the accumulation of trash and debris.

The visual impact of sidewalk paving should be broken up to give it a sense of scale. A grid of joint lines (maximum 2-4 feet) achieves this as well as bricks or pavers which provide sidewalk texture.



**Appropriate:** Gridded concrete sidewalk with street trees in square grates and a bike rack



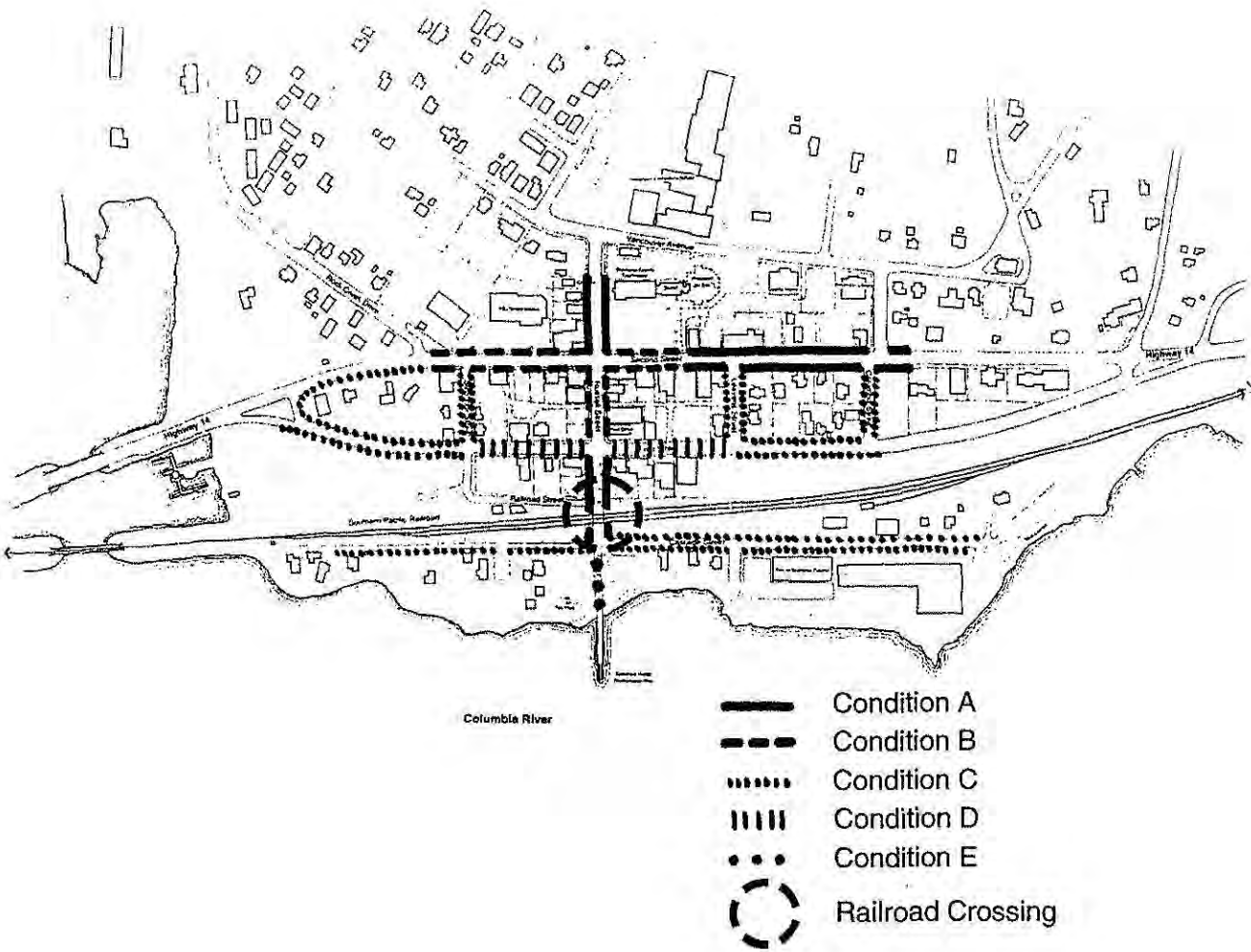
**Not Appropriate:** Narrow sidewalk without an adequate area for street trees



2019 Interim Zoning  
Control Annotations

This map does not include the actual improvements made within the planning areas since 1999. New sidewalks and sidewalk improvements within all areas of Zones 1 through 3 should be considered on a case-by-case basis, especially for those corridors identified on the map on Page DF-8.

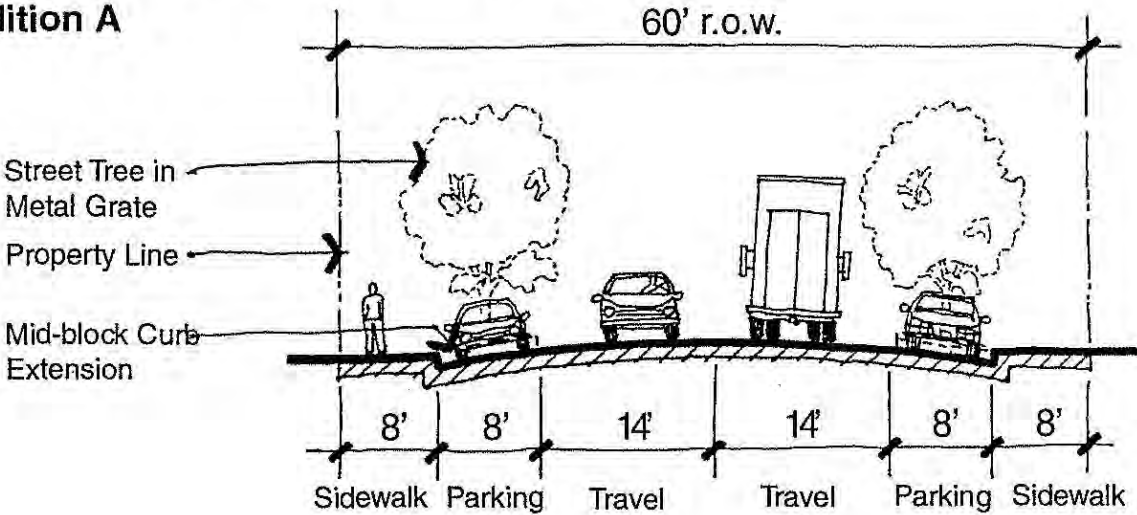
Sidewalks



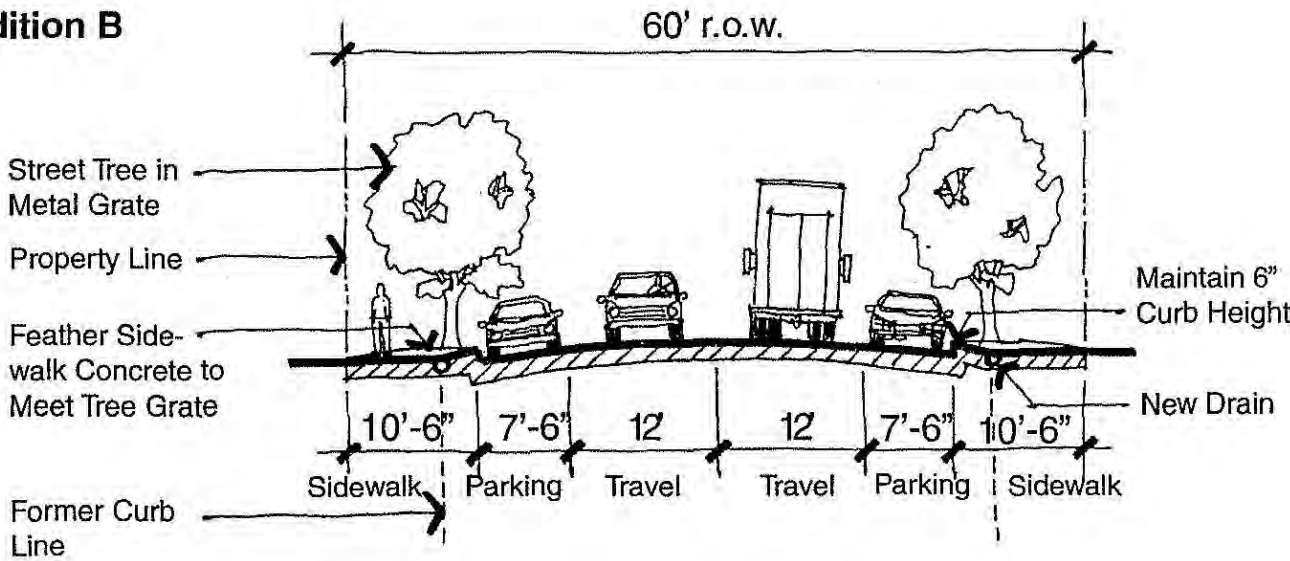
See also the As-Built construction documents  
for the SR14/2nd Street Couplet Upgrade  
project.

Sidewalks **\*\*44' typical curb-to-curb width to be verified**

Condition A

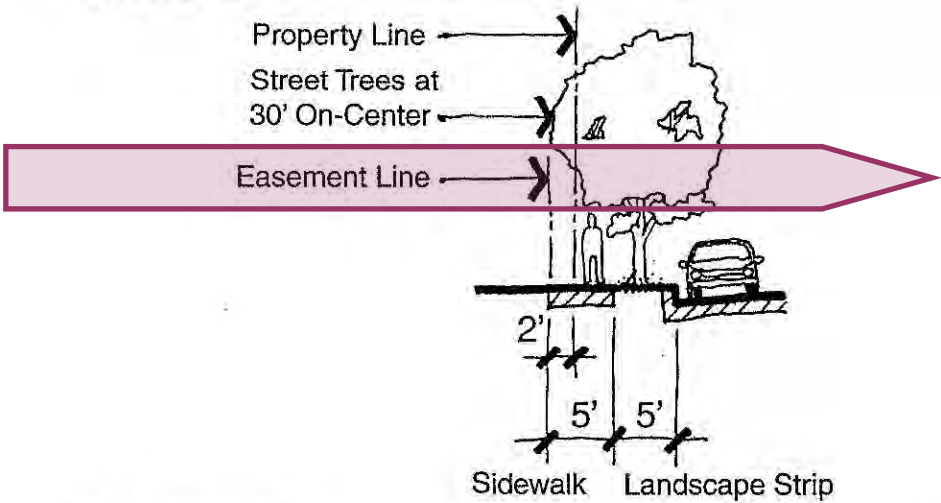


Condition B

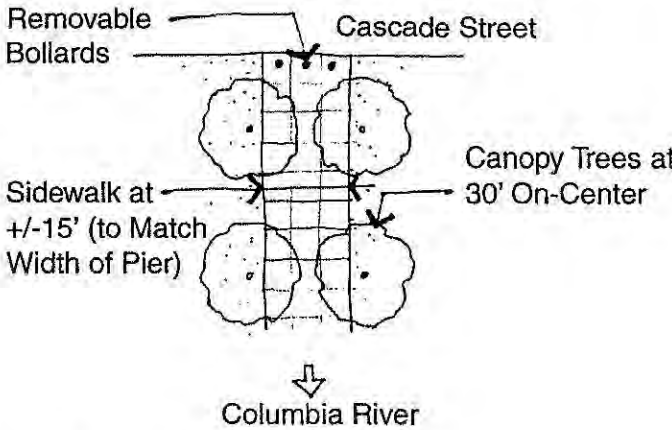


Sidewalks \*\*44' typical curb-to-curb width to be verified

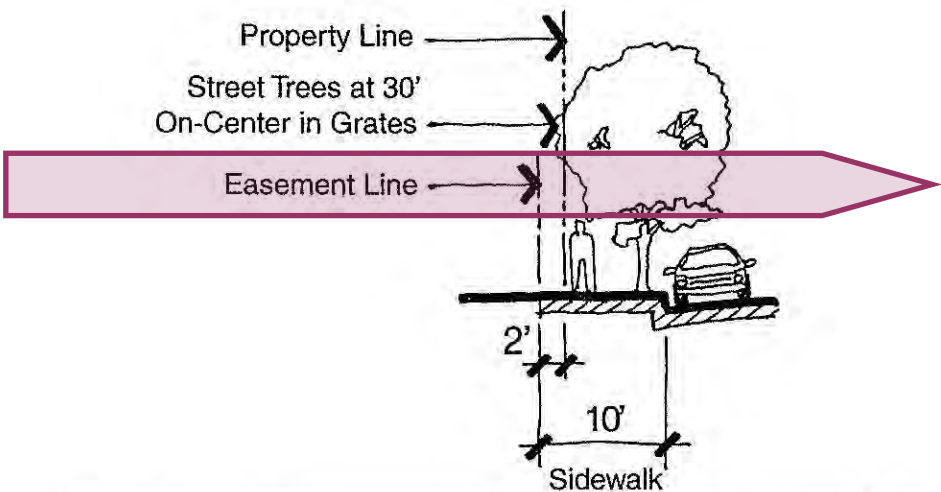
Condition C



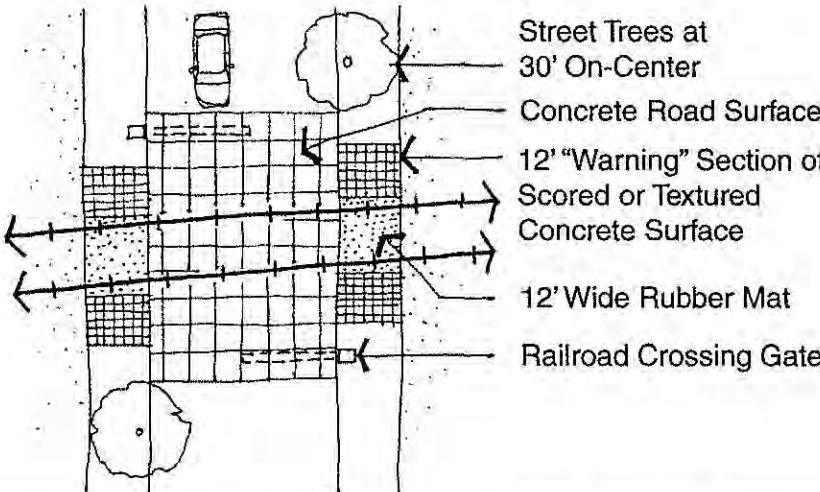
Condition E



Condition D



Railroad Crossing



The easements for sidewalks as shown on this page are important contributors to the Development Framework's concepts for Build-To Lines, Active Street Frontages and Rain Protection.

If a project is proposed along a sidewalk without sufficient width to achieve the Development Framework's objectives, it shall either:

- Provide sufficient easement to the city and construct the sidewalk,
- Construct the sidewalk within the existing right-of-way after obtaining approval to reduce the width of the parking and/or travel lanes of the adjacent roadway, or
- Demonstrate to the satisfaction of the Planning Commission that failing to undertake the above will not harm the public's interest.

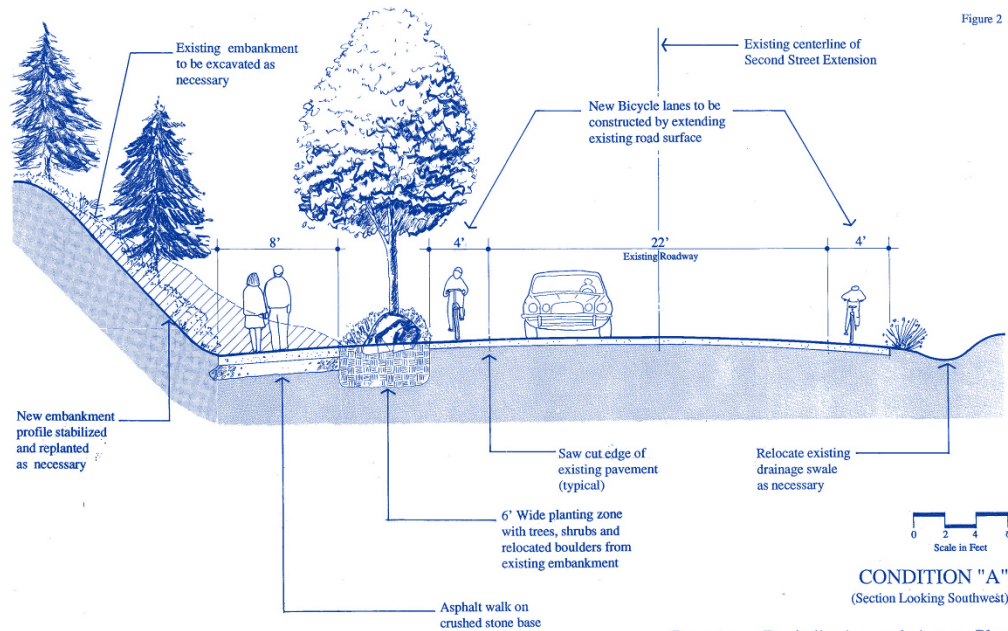
See also Pages DI-2 and DI-3 for development incentives associated with pedestrian amenities.



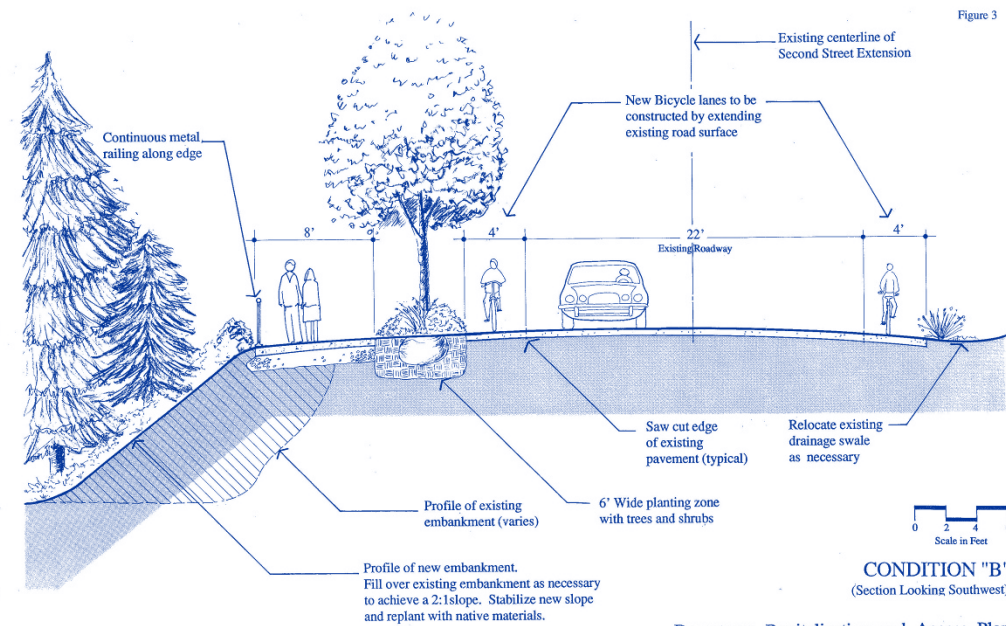
# 2019 Interim Zoning Control Annotations

## DESIGN GUIDELINES - PUBLIC AREAS

## DOWNTOWN STEVENSON



Downtown Revitalization and Access Plan  
Stevenson, Washington

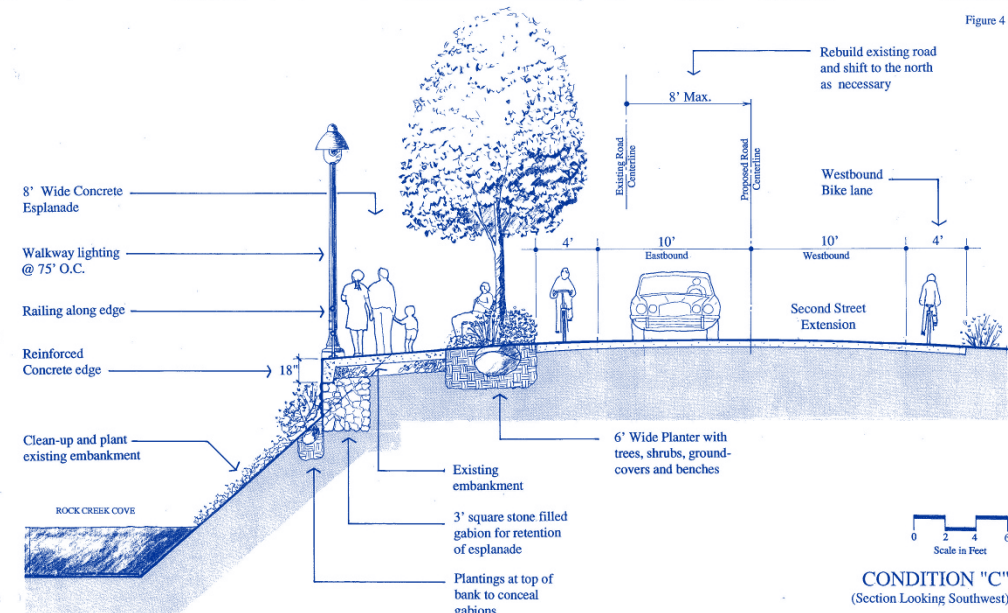


Downtown Revitalization and Access Plan  
Stevenson, Washington

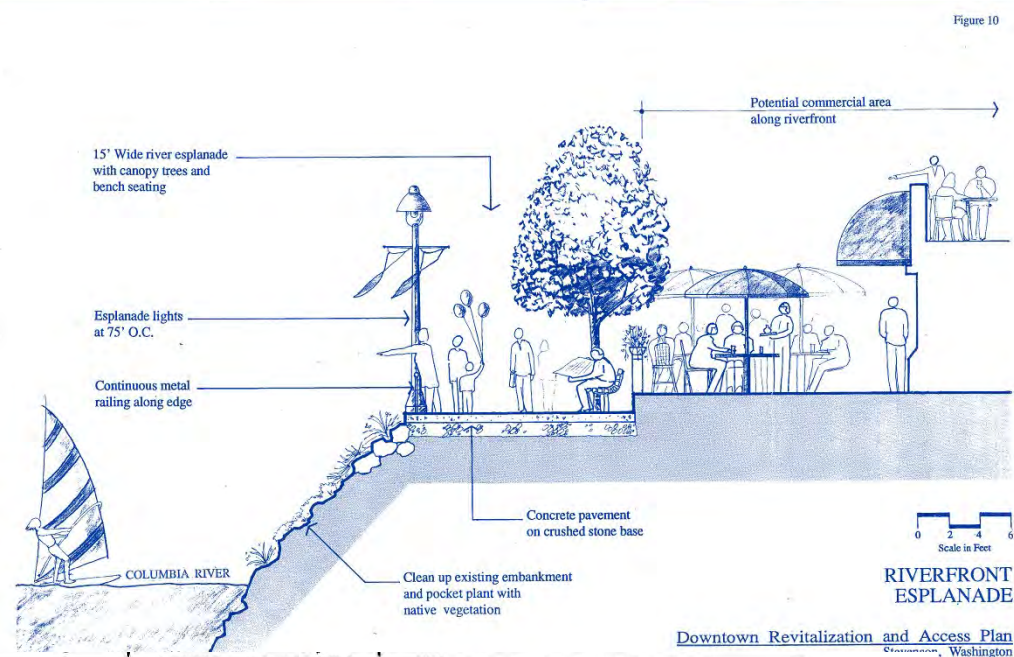
These figures illustrate concepts from the map extension on Page DF-4

All are taken from the 1991 *Stevenson, Washington Pedestrian and Bicycle Links* prepared by Walker & Macy.

See Page PA-10 for development of sidewalks along these corridors.



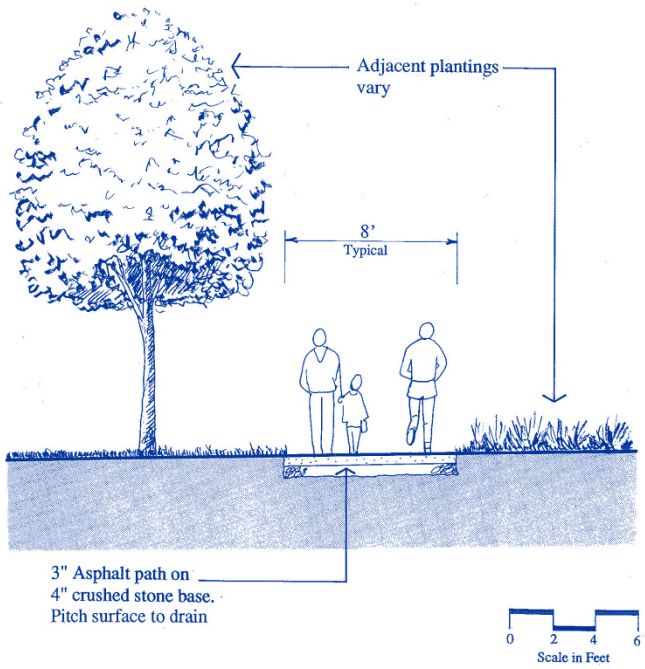
Downtown Revitalization and Access Plan  
Stevenson, Washington



Downtown Revitalization and Access Plan  
Stevenson, Washington



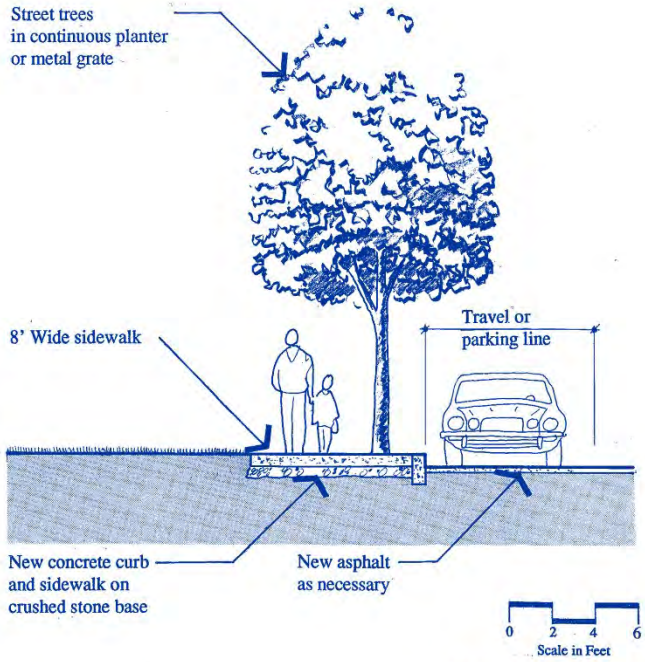
Figure 5



PHASE I PATH  
(Typical)

Downtown Revitalization and Access Plan  
Stevenson, Washington

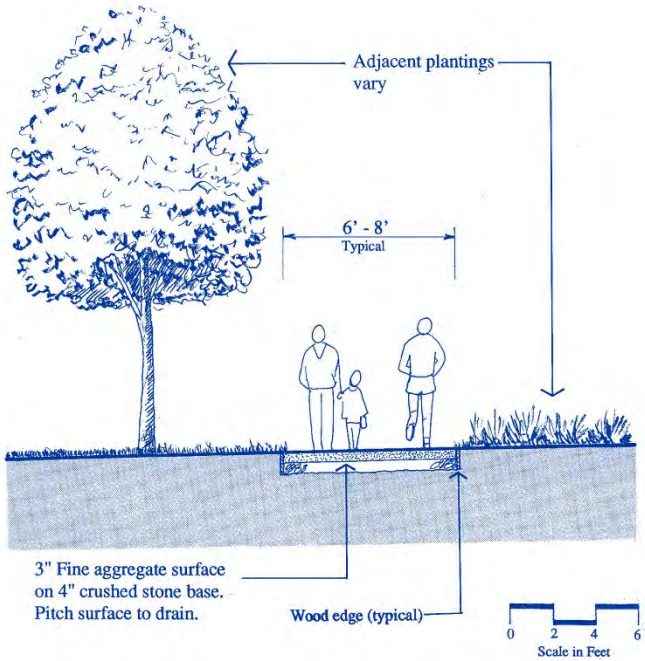
Figure 7



Phase I Sidewalk  
(Typical)

Downtown Revitalization and Access Plan  
Stevenson, Washington

Figure 9



SECONDARY PATH  
(Alternative Paving)

Downtown Revitalization and Access Plan  
Stevenson, Washington

These figures illustrate concepts from the map extension on Page DF-4

All are taken from the 1991 *Stevenson, Washington Pedestrian and Bicycle Links* prepared by Walker & Macy.

See Page PA-10 for development of sidewalks along these corridors.

No annotations to this page.

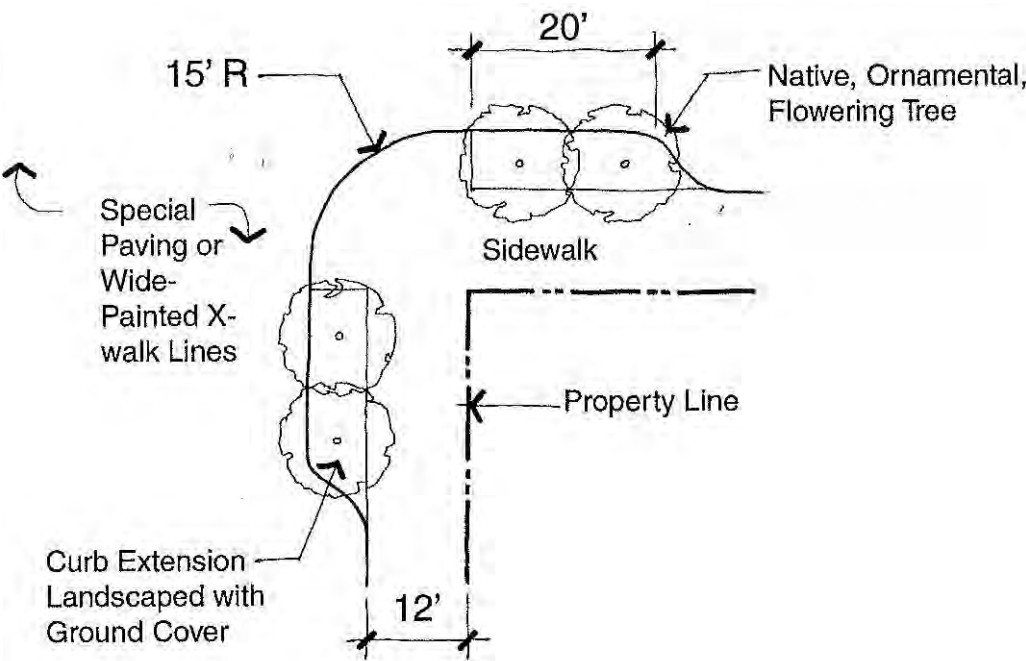
Curb Extensions

Intent

Curb extensions are an important part of improving the downtown environment for pedestrians. Extending the curb edge reduces the crossing distances for pedestrians (especially important for children and the elderly). Extensions walking “loops or circuits” within downtown, thereby encouraging shoppers to cross the street and patronize other businesses on both sides of the street.

The curb extension must include ornamental, flowering street trees and low shrubs, creating special landscaped intersections. The trees, along with the narrowed roadway at the intersections acts as a traffic-calming device. Drivers perceive a tighter space to pass through and slow down while passing through an intersection with curb extensions on all four sides. Landscape materials must be carefully selected to avoid impacting sight lines.

Curb Extension Detail





No annotations to this page.

# Pedestrian Street Crossing

## Intent

Crosswalks should minimize the intersection as a barrier to pedestrian movement. The street crossing should be clearly defined and highly visible. The crossing should be a minimum of 10' in width to accommodate numerous pedestrians.

## Appropriate:

- Wide striped, reflective painted or applied strips parallel to roadway
- Special paved brick or tinted concrete surfaces
- Durable and low maintenance materials

## Inappropriate:

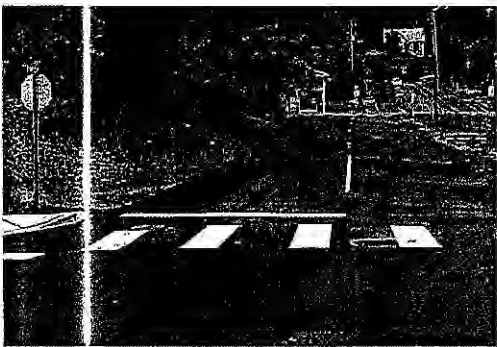
- Deeply scored or textured materials that may impede those who are physically challenged
- Simulated brick or stone from stamped and tinted concrete appears inauthentic, lacks durability, typically provides a lumpy surface that is unsuitable for pedestrian crossings, and is not compatible in a historic district.



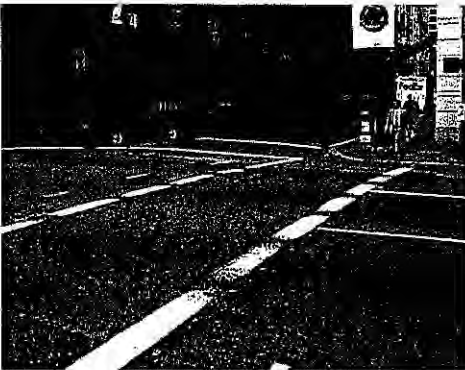
**Appropriate:** Brick sidewalks clearly delineate the automobile zone vs. the pedestrian zone.



**Not Appropriate:** Simulated brick or stone from stamped and tinted concrete



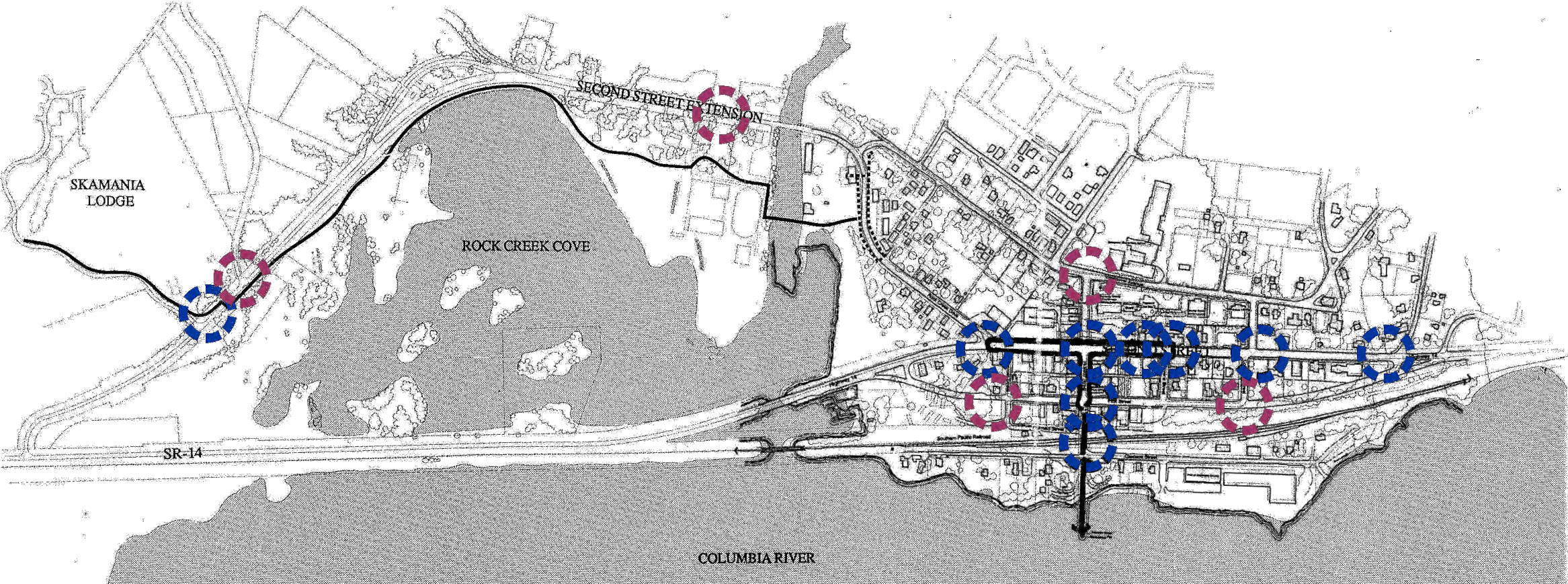
**Appropriate:** Wide stripes parallel to traffic movements reinforce the zone reserved for pedestrians






**Not Appropriate:** Single stripes wear out in heavy traffic and only minimally define the pedestrian zone



Pedestrian Street Crossing



-  Special Pedestrian Crossing
-  Installed Since 1999
-  Planned Since 1999



## Street Furniture

### Intent

Street furniture should be provided to encourage activity and interaction. Along sidewalks and public spaces, areas should be reserved for artwork, benches, drinking fountains, bike racks, and transit shelters. Street furniture should be located only within the designated sidewalk zones. It should not block, impede or discourage pedestrian activity.

### Appropriate:

- Durable, low maintenance materials
- Historically compatible with downtown architecture
- Dark green or black graffiti-resistant paints

### Inappropriate:

- Advertising on street furniture
- Synthetic or plastic poles, standards or benches
- Bicycle racks with movable or operable components

### Benches



**Appropriate:** Bench of weather-resistant metal



**Appropriate:** Wood and iron bench in a historic style



**Not Appropriate:** Contemporary forms



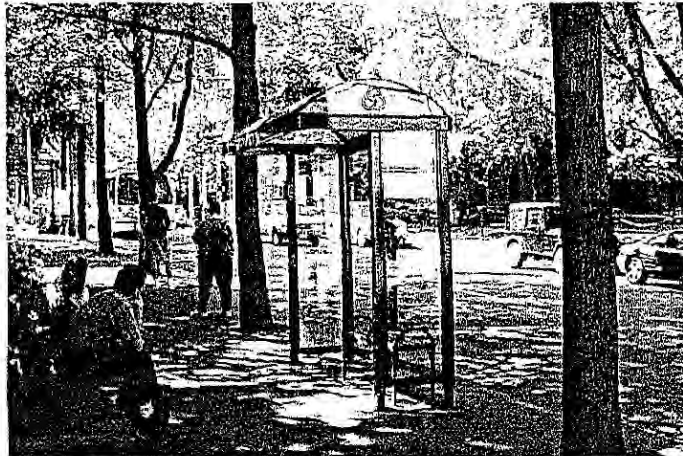
**Not Appropriate:** Bench without back

•Inappropriate: Benches without interior armrests, which provide personal space for individual occupants from different groups.



## Street Furniture

### Transit Stop

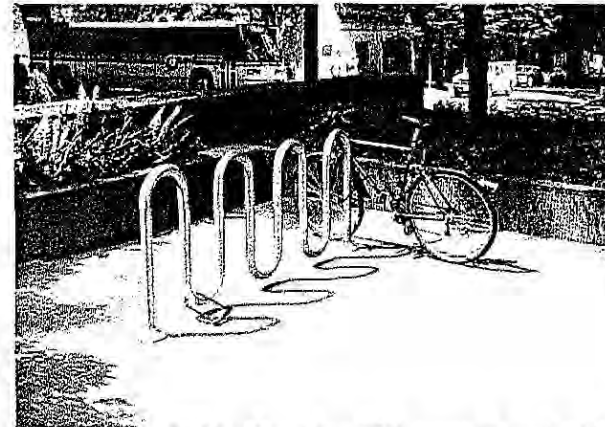


**Appropriate:** Transit stop with bench and snow, rain, and wind protection



**Not Appropriate:** Transit stop with no weather protection

### Bike Racks



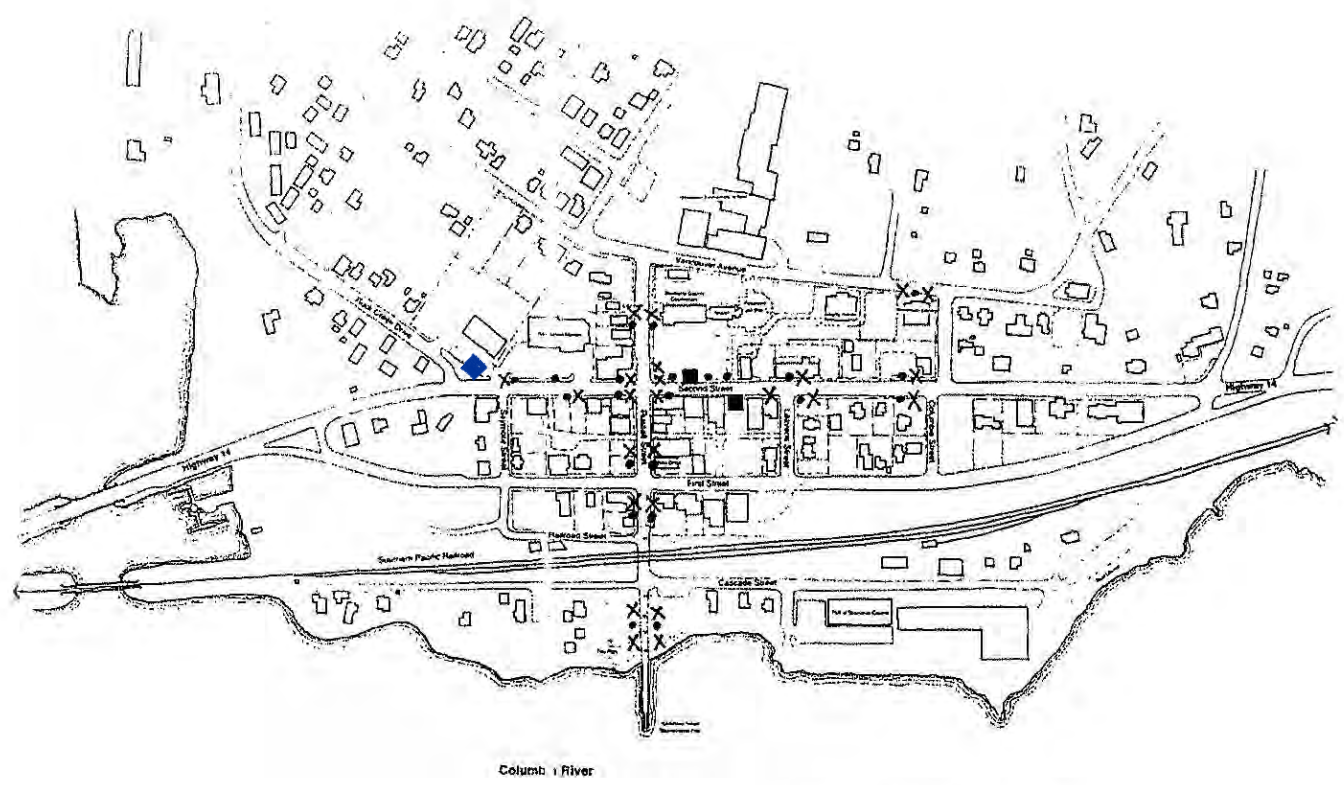
**Appropriate:** "Ribbon" rack has high capacity and, with space provided



**Not Appropriate:** Racks with moving parts are often confusing and present a maintenance problem

For the specifications used for the transit shelters and bike racks installed since 1999, contact City Hall.

Street Furniture



- Transit Shelter
- X Bike Rack
- Bench
- Installed Since 1999
- Transit Shelter

2019 Interim Zoning Control Annotations

This map does not include the improvements made to or planned for Rock Creek Drive. New street furniture within Zones 1 through 3 areas should be considered on a case-by-case basis.



*No annotations to this page.*

## Railings & Fences

### Intent

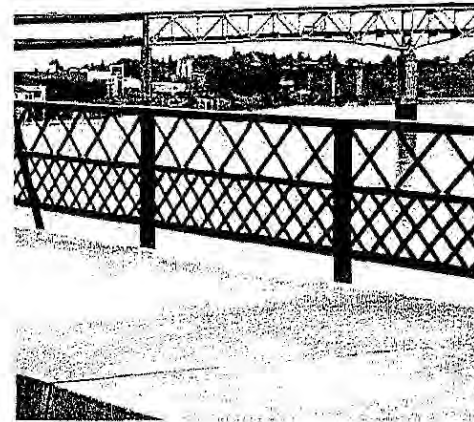
Railings and fences should be used sparingly as design features. They should be used only when required by code or to create or contribute to a visual screen between incompatible uses.

### Appropriate:

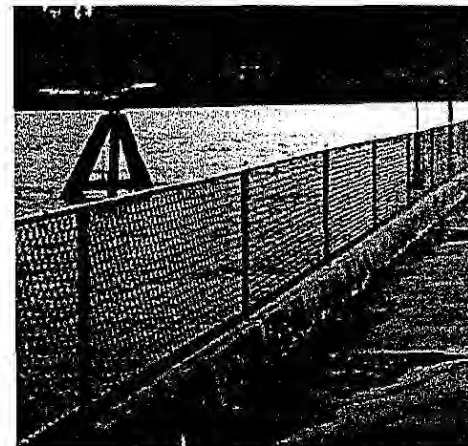
- Ornamental painted (dark green or black) fences
- Wood (with Craftsman architecture only)

### Inappropriate:

- Chain-link fence



**Appropriate:** Lace-like Steel balustrade



**Not Appropriate:** Chain-link fence on top of concrete curb



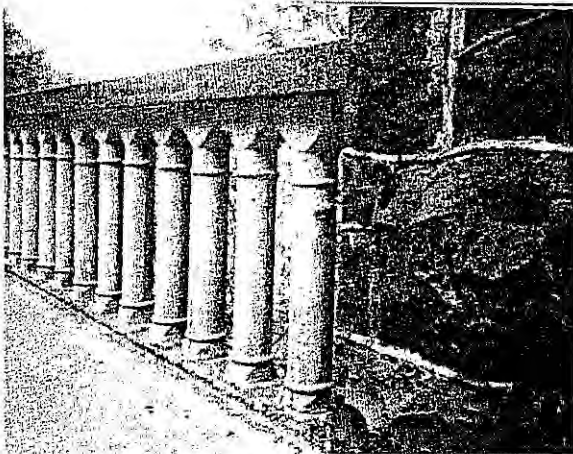
# Retaining Walls

## Intent

Because of the sloped nature of downtown Stevenson, retaining walls are likely to be required for many developments. The retaining walls should be consistent with the construction technique, materials of the WPA stone work common throughout the Columbia Gorge.

## Appropriate:

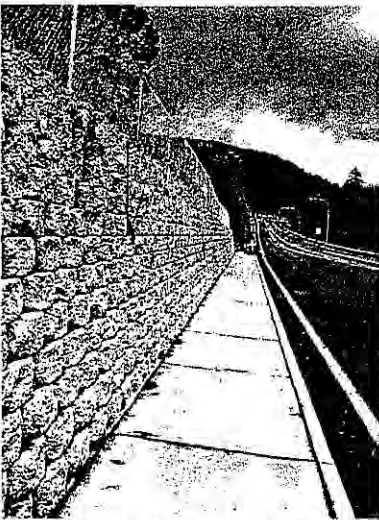
- Indigenous Stone (basalt)
- Integration of concrete pilasters or ballusters
- Integration of light fixtures or wrought-iron fences



Appropriate: indigenous stone retaining walls

## Not Appropriate:

- Block retaining wall
- Simulated stamped-stone concrete walls
- Precast concrete wall systems
- Wood ties



Not Appropriate: Keystone block retaining walls

•City-approved murals on the face of concrete retaining walls.

•Blank concrete retaining walls

## **Design Guidelines - Private Development**

Purpose

Building Character

Building Form

Windows

Roofs

Walls

Entries

Colors

Sign Concept

Applied Signs

Hanging Signs

Prohibited Signs

Lighting Concept

Exterior Building Lighting

Parking Lot Lighting



## Purpose

The private development guidelines ensure that each development site is consistent with the development framework. They express the desired character of future development.

The guidelines recognize and foster maintaining downtown Stevenson as an urban place of buildings, in most instances, built edge to edge and engaging the street. The guidelines promote new construction and rehabilitation that embraces the many unique qualities of the existing urban development.

Buildings should be “good neighbors.” They should be compatible with surrounding buildings by avoiding disruptive excesses. A compatible building should relate to nearby buildings’ scale color, rhythm and proportion.

The Design Guidelines are not retroactive and apply only to new construction, remodeling, and changes of use. For example, an existing business with signs not in compliance with the Design Guidelines would not be required to change the signs at the time the Guidelines become effective. However, if the existing business were to replace the signs after the Design Guidelines come into effect, the new signs would be required to comply. Existing uses and structures are “grandfathered in” in this manner over time.

## Architectural Integrity

Development in Downtown Stevenson may involve a great deal of rehabilitation, remodeling or additions to existing structures. The following are minimum standards that should be considered to ensure that the rehabilitation of existing buildings preserves the character downtown.

- Buildings additions should not deform or adversely effect the composition of the facade or be out of scale with the building.
- Historic buildings should be respected and recognized as products of their time. Distinctive, stylistic features such as wood detailing and trim at openings, eaves and cornices, or examples skilled craftsmanship such as quality wood windows and doors should be treated with sensitivity. For example, finely crafted wood windows and doors should either be repaired and reused, or replaced with similar finely crafted substitutes.

## Private Development Guideline Goals

Private development decisions must be made with awareness of their urban design consequences. Development should:

- Use quality materials that promote permanence and delight.
- Contribute to a strong organizing pattern. The sum is more important than one individual building.
- Balance functional concerns, efficiency, and cost of construction with the role of the building in the cityscape and the definition of streetspace.

A compatible building should relate to nearby buildings’ scale, color, rhythm, proportion, and orientation.



# Building Character

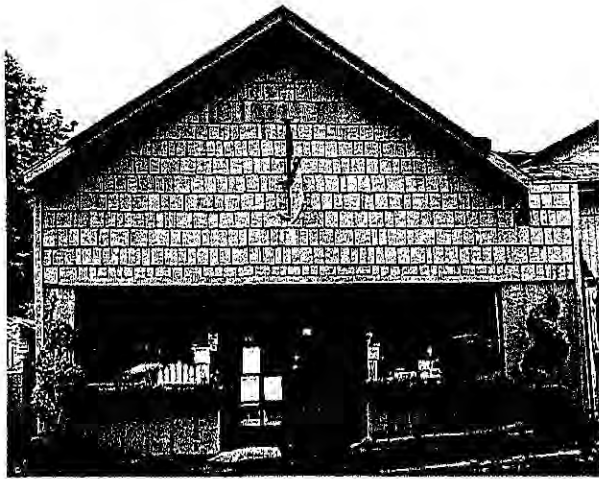
The standard promotes architectural elements that unify downtown Stevenson. The architecture should reinforce Stevenson's riverfront development era that occurred generally from 1900 to 1935. The guidelines promote only architectural types that are historically indigenous to Stevenson or the Columbia Gorge area. "Placeless" architectural styles, typical of national chain stores are inappropriate. A small palate of architectural styles is suggested to maintain unity and provide variety.

## Appropriate:

- Simple-Unadorned
- Cascadia- Lodge-like
- Craftsman

## Not Appropriate:

- Anonymous, slick-skinned contemporary architecture lacking richness and detail
- Placeless, nationwide corporate chain store architecture



Appropriate: Simple unadorned buildings



Not Appropriate: "Placeless" corporate identity

...The guidelines promote architectural types that are historically indigenous to Stevenson or the Columbia Gorge area. When used in the context of these interim zoning controls, the guidelines must incorporate the pleasant aspects of more recent development which has become a characteristic of downtown Stevenson today. "Placeless" architectural styles, typical of national chain stores are inappropriate. While a small palate of architectural styles is suggested to maintain unity and provide variety, the Design Review process should be very accepting of other styles during the effective period of these interim controls.

# Building Form

Buildings in downtown Stevenson should be consistent and complementary to the fabric of early-20th century buildings that still remain.

**Appropriate:**

- New buildings of brick and wood are most appropriate, while the use of steel, plastics and concrete on the exterior should be avoided.
- Buildings downtown occupy small sites. New developments that occupy more than a quarter of a block should be broken up to appear as more than one structure.
- Heights of new buildings should not vary more than 25% from the two story height of many existing buildings in the downtown. Structures beyond three stories are incompatible and could overwhelm the scale of downtown.
- Buildings should provide spatial definition for pedestrians along the base of the building. At this street level, buildings should have indentations, projections and rich texture.



**Appropriate:** Two-story brick building



**Appropriate:** Two-story, mixed-use wood-panel building



**Appropriate:** One-story wood frame building



**Not Appropriate:** Contemporary in form

- Heights of new buildings should incorporate a second story similar to many of the existing mixed use buildings characteristic of the riverfront development era. Structures taller than the Avary Building are incompatible and could overwhelm the scale of downtown.
- To maintain the human scale, new development taller than one story should incorporate a cornice line or similar horizontal element that is within 25% of similar elements on buildings adjacent to or on the same block as the proposal.



## Windows

Within downtown, only high quality, historically appropriate windows and window materials are suggested. Low quality “reproduction” windows that create a building composition that appears inauthentic or has a “stage-set” appearance are inappropriate. Openings in the exterior walls of buildings add variety and interest at the street level. Business’s prosper as the transparency created at the ground floor invites people in. Restaurants and cafes can benefit even more if they have window that can actually open -- by sliding, pivoting, or shuttering. Windows makes it possible for pedestrians to see, hear, and smell what the business has to offer.

### Appropriate:

- Multiple small panes
- Large plate glass (for views)
- Dark wood or metal sashes
- Transparent glazing
- Transom windows
- “Punched”, recessed window openings

### Not Appropriate:

- Large plate glass (as primary architectural feature)
- Simulated, divided lites
- Single, small panes
- Windows flush with building wall
- Reflective, opaque or tinted glazing at street level



Appropriate: Operable Plate Glass Windows



Not Appropriate: Small, fixed windows

...Restaurants and cafes can benefit even more if they have windows that can actually open—by sliding, pivoting, **rolling up**, or shuttering...



*No annotations to this page.*

## Roofs

Because Downtown Stevenson is on a hillside, appropriate rooftop treatment is important to avoid adversely impacting surrounding buildings and open spaces.

“Functional Roofs” that are consistent with the suggested architectural styles are appropriate. The roofs in downtown should be calm, consistent, natural and subdued. The roof should not be the primary architectural feature.

Roof mounted mechanical equipment should be hidden from view by parapets. A parapet is a low, protective wall built up along the edge of a roof (an example is at the Rainy Day Gallery and Studio). If building parapets cannot provide adequate screening, mechanical equipment should include walls or enclosures installed as an integral part of the architectural design of the building.

### Appropriate:

- 4:12 minimum pitch
- 12:12 Maximum pitch
- Small Dormers - 4:12 Minimum Pitch
- Flat or low pitch screened by parapet walls

### Materials:

- Cedar Shingle
- Three Tab
- Metal (corrugated or small folded, standing seam)
- Muted Dark Colors- grays, blacks, dark green, dark brown

### Not Appropriate:

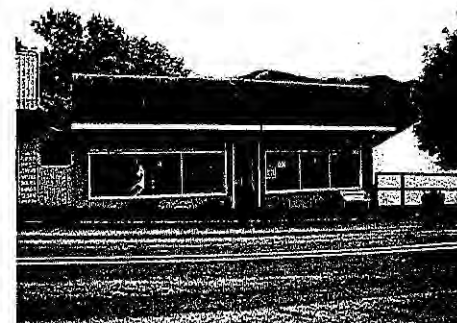
- Nonfunctional decorative roofs
- Brightly Colored Roofs (red, green, blue, tan colors)
- Mansard or other applied decorative roof forms



**Appropriate:** Dark metal, gable ended roof



**Appropriate:** Small dormers



**Not Appropriate:** Mansard or other applied decorative roofs

No annotations to this page.

Walls

Historic or authentic-appearing exterior cladding materials should be used. Wall materials and application techniques that are consistent with the building character should be fostered. Long lasting materials that are durable and require little maintenance should be employed.

Appropriate:

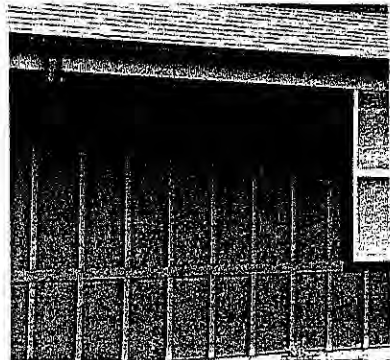
- Smooth Bevel Drop Siding
- Shingles
- Vertical Siding (Wood Board & Batten)
- Brick
- Native (Basalt) Stone Masonry

Not Appropriate:

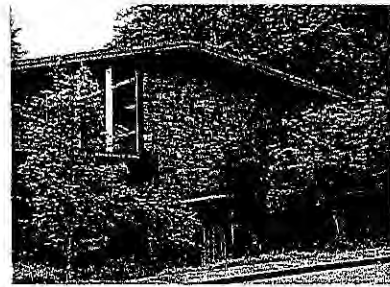
- Metal Panels
- Wood Shakes
- Exposed Concrete Block
- Flagstone or Other Applied Stone Veneer Products
- Large Precast Concrete Panels
- Cast-in-place Concrete
- Plywood Panelling (T-111)
- Synthetic Cladding Materials at First Floor (Stucco/ Fiberglass Panels)



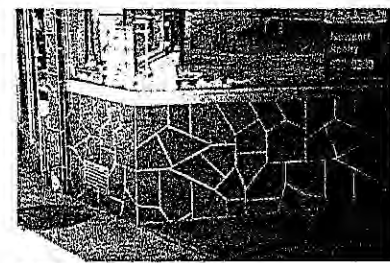
Appropriate: Cedar shingles



Appropriate: Board and Batten



Appropriate: Native stone (basalt) masonry



Not Appropriate: Flagstone Veneer

## Entries

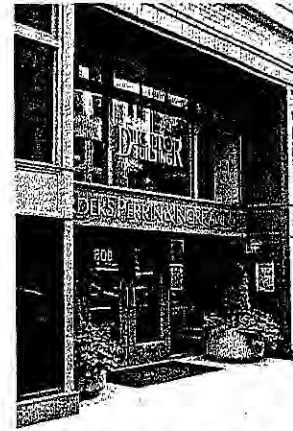
Doorways should be located so that buildings appear welcoming and inviting. Corner entries and recessed doorways are suggested. Retail shops and businesses should have individual entries from the street rather than from interior spaces such as lobbies or atriums.

### Appropriate:

- Wood
- Recessed
- Transparent with single or multiple lites
- Corner

### Not Appropriate:

- Large glass doors
- Security gates
- Solid wood or metal doors
- Overhead garage doors
- Aluminum frame
- Entryway plazas or forecourts



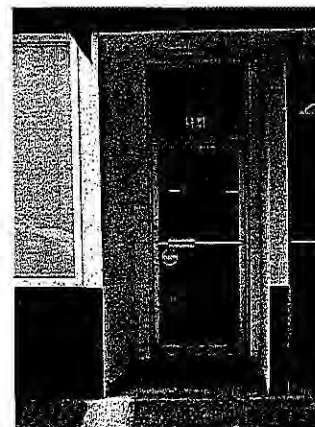
Appropriate: Recessed doorway



Appropriate: Wooden door frame



Appropriate: Wooden door frame with wood detailing above



Not Appropriate: Thin, aluminum frame in narrow recess



Not Appropriate: Overhead Garage doors engaging public streets

- Roll-up garage doors that include more than 50% transparent paneling at the pedestrian level.



*No annotations to this page.*

Colors

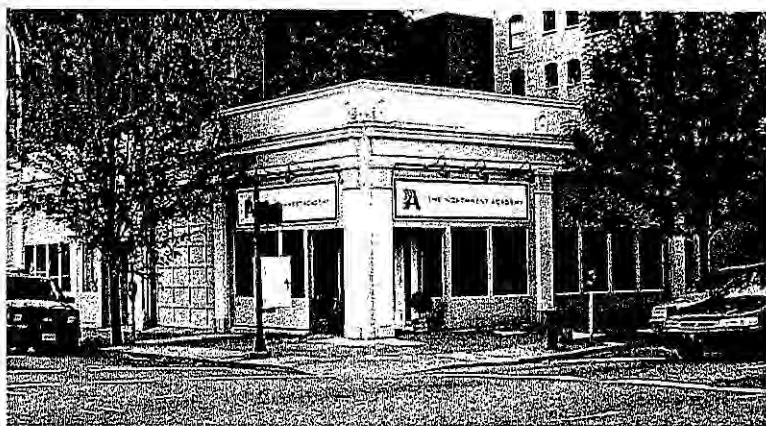
Only building colors that are consistent with the architectural character or suggested palate of architectural styles should be used.

Appropriate:

- Shades of white
- Natural and “weather accelerating”
- Dark color doors, windows and trim

Not Appropriate:

- Bright primary colors
- Untreated wood



Appropriate: Shades of white



Not Appropriate: Bright blue

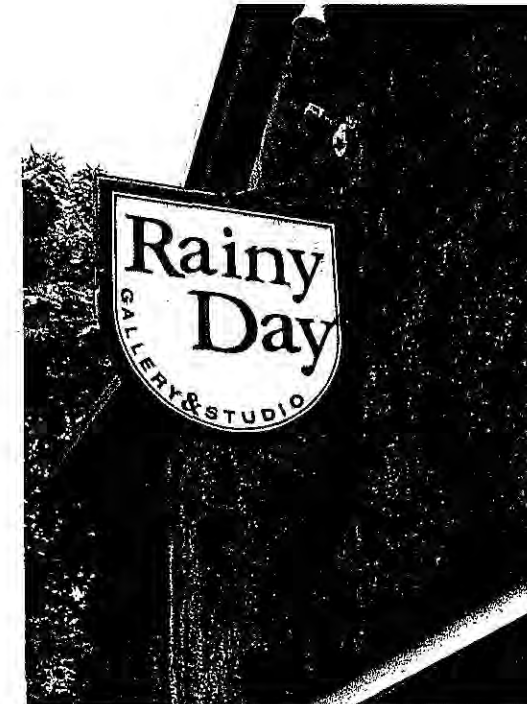
## Sign Concept

The guidelines identified herein supercede existing ordinances. In instances where no guideline is provided, sign proposals must meet the standards and requirements of the Stevenson's Zoning Ordinance. Advertising, construction, political or portable signs must be consistent with the City's Zoning Ordinance regulations. These guidelines apply to permanent building, tenant or informational and temporary signs only.

Public events are best advertised on banners affixed to street light standards. Design standards for banners should be established. Applications and concise plans for banners, setting forth dimensional requirements, materials of construction, mounting heights, color and graphic format shall be subject to review and approval. An additional means of advertising public events and information would be to post approved material at a kiosk located in the proposed courthouse plaza.

Signs may provide an address, identify a place of business, locate tenants, reinforce the downtown's character or generally provide directions and information. Regardless of function, signs should be architecturally compatible and contribute to the character of the area. Signs should not compete with each other or dominate the setting due to inconsistent height, size, shape, number, color, lighting or movement.

Signs should not overwhelm the building or the landscape. They should instead provide easily recognizable business identification and information in a highly graphic format that is complementary to the Downtown Stevenson's architecture. Tasteful logos, symbols and graphics are encouraged. A strong pedestrian orientation is encouraged.



Signs should be highly graphic and oriented toward the pedestrian

Each development or building represents only a small portion of the downtown as a whole, but contributes significantly to the overall visual image of downtown. The uniform application of sign guidelines addressing type, location, size and quality will ensure a visually pleasing downtown environment.

## 2019 Interim Zoning Control Annotations

This component of the 1999 plan was partially incorporated into the City's Zoning Code through an amendment in 2011. Those standards remain in effective during the effective period of these interim zoning controls. However, major projects may request to deviate from the standards of the Zoning Code as part of the project's design review. Such requests must be clearly identified and should use the submittal requirements of SMC 17.39.145 for guidance. Proponents requesting a deviation shall demonstrate to the satisfaction of the Planning Commission that the proposal is consistent with these guidelines.



## Applied Signs

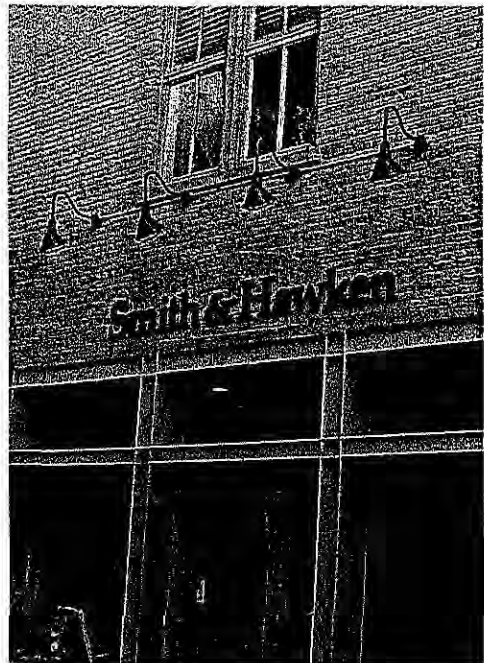
Applied wall signs can be located along the top, middle or at the pedestrian level of buildings. Signs can be incorporated into the building architecture as embossing or low-relief casting. They may be painted, or applied metal lettering and graphics. Signs should be durable and long lasting. They may incorporate lighting as part of their design (see guidelines in Lighting). Signs should identify the building or business's name and should not be used to advertise products or services.

### Appropriate:

- Front-lit
- Historic in character
- Modest scale

### Not Appropriate:

- Plastic
- Backlit of any material



Appropriate: Front lit metal wall signs



Not Appropriate: Internally lit plastic sign

[See Page PD-9.](#)



## Hanging Signs

Hanging signs should be very graphic and constructed of high-quality materials and finishes. They should be attached to the building with durability in mind. Signs should be compatible and complement the building's architecture, awnings, canopies, lighting, and street furniture.

Hanging signs should be highly visible from the sidewalk. Generally, they should be attached perpendicular from the building, or at 45 degree angles from corners.

### Appropriate:

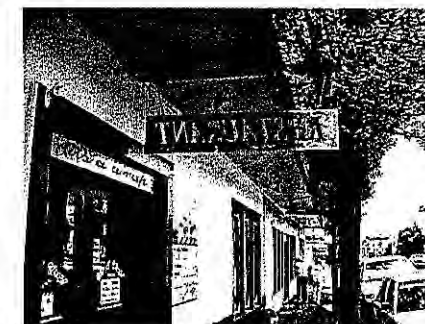
- Small Scale (9 sq. ft. recommended, maximum 15 sq. ft.)
- Historic in character
- Front lit

### Not Appropriate:

- Backlit of any material



**Appropriate:** Graphic hanging sign



**Not Appropriate:** Difficult to read hanging sign

[See Page PD-9.](#)

See Page PD-9.

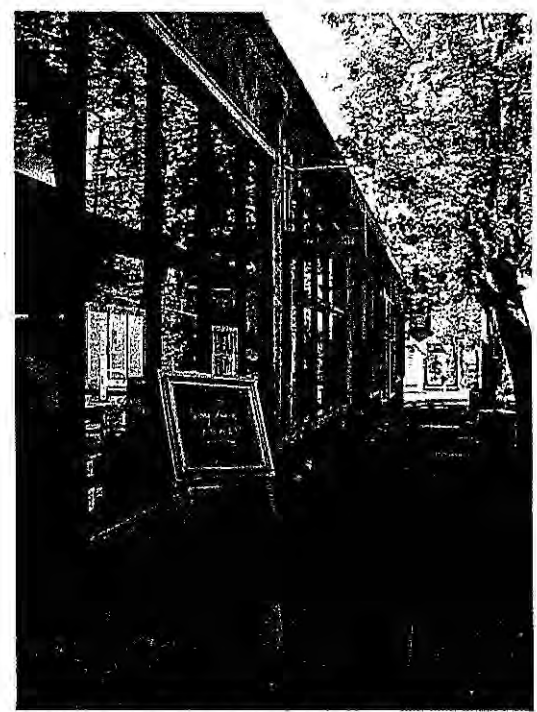
# Temporary Signs

Signs identifying uses or activities temporary in nature, such as “sandwich board” signs or signs associated with real estate sales and leasing and the development and construction of buildings, shall be allowed on a temporary basis if consistent with the design character of the surrounding area.

Temporary signs must be located on or in close proximity to the uses identified.

The large number of signs often associated with development, design, construction and leasing shall be combined into one sign for each project and the sign shall be located on site.

Applications and concise plans for temporary signage, setting forth dimensional requirements, materials of construction, mounting heights, color and graphic format shall be subject to review and approval.



Appropriate: Temporary sign consistent with the design character of the surrounding area

## Prohibited Signs

- Exterior neon
- Monument signs
- Freestanding pole signs and roster boards
- Moving or animated signs that create traffic hazards
- Obsolete or abandoned signs (remove within 30 days of business closure)
- Temporary signs on public property including signs on traffic signals, utility poles, trees, etc.
- Vehicle or portable signs
- Off-premise signs
- Rooftop signs
- Inflatable or balloon signs
- Magnetic signs
- Painted window advertising signs
- Plastic flags
- Signs in poor repair



**Not Appropriate:** Moving or animated signs

[See Page PD-9.](#)

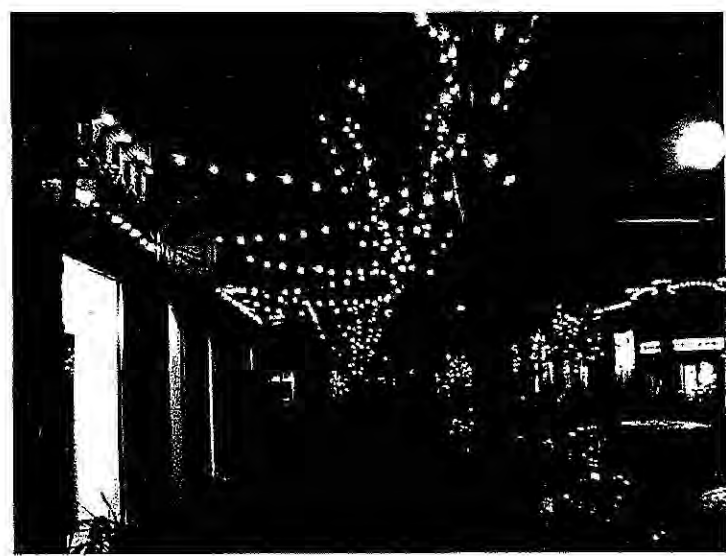


*No annotations to this page.*

Lighting Concept

Lighting should not only provide nighttime security, but also encourage nighttime patronage of businesses and restaurants. Lighting of special elements should create an atmosphere of festivity and activity. Utilitarian applications of glaring, or offensively-colored lights should be prohibited.

Parking lot, building, and pedestrian lights play a vital role in defining a quality appearance within downtown. The lighting concept has been devised to provide a hierarchy of lighting effects that will contribute to an overall unified downtown image.



*No annotations to this page.*

## Exterior Building Lighting

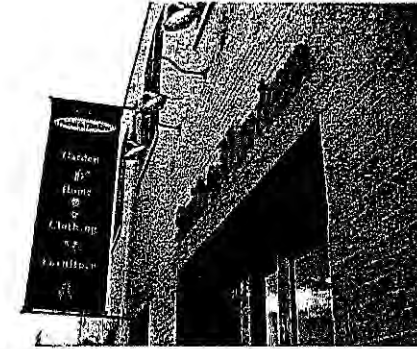
Architectural lighting should be integrated with building elements and landscaping. A limited number of lights may be used to create shadow, relief, and outline effects. Lighting should be used for wall washing, wall highlighting, sign illuminating, and be directed towards the building, not the public right-of-way. Lighting should not cast any glare onto adjacent lots in such a manner as to decrease the safety of the pedestrian, vehicular movement or effect the livability of nearby residents or businesses.

### Appropriate:

- Decorative, historic wall sconce and similar architectural lighting fixtures.
- Metal Halide or other natural color spectrum lamp
- "Gooseneck" or similar architectural lighting fixture
- Integrated and screened uplights or spotlights

### Not Appropriate:

- Sodium
- Neon
- Fluorescent Tube
- Broadcast Lighting
- Flashing Lighting



Appropriate: "Goose neck" lighting



Appropriate: "Goose neck" lighting

*No annotations to this page.*

## Parking Lot Lighting

Parking lot lighting should clearly define accessways to streets and building entries. It should be directed downward so that it does not produce glare into surrounding areas. Historic ornamental fixtures that are in character with downtown architecture and are compatible with the street furniture should be used. Pole standards should be black or very dark green color. Parking lot lights should be integrated into landscape islands. Building security lights (floodlights) should not be used as substitutes for parking lot lighting. The lighting should be bright enough to provide a sense of security and safety.

### Appropriate:

- Historic fixtures
- Pedestrian scale
- Dark pole standards

### Inappropriate:

- Cobra-head type fixtures
- Contemporary fixtures
- "Broadcast" security lighting
- Motion-detecting security lighting
- Lighting recessed in bollards or other lighting at low heights



## **Development Standards**

Background

Key Distinctions: Development Framework & Standards  
and Existing Code

Existing Land Use Zones and Proposed  
Development Zones

Development Standards Matrix

Development Standards Matrix Notes

Applying the Development Standards

Background

Downtown Stevenson is divided into two development zones, Zone 1 and Zone 2. Located within Zone 1 are the two primary retail streets that hold the key to success for the downtown. Adherence to the guidelines within Zone 1 is paramount for this is where the majority of pedestrian activity will occur. Zone 2, on the other hand, allows more flexibility for uses and does not include the primary active streets in the downtown.

Zone 1 is designated as reaching 100 feet deep on each side along Second Street from Rock Creek Drive to Columbia Street and Russell Street from Vancouver Avenue to Railroad Street. Zone 2 includes the area between the confluence of First and Second Street at their east and west ends, Vancouver Avenue to the north and the Columbia River to the south.

The Development Standards supercede the existing Development Code where discrepancies exist. They are regulatory requirements which are not negotiable during the Design Review Process.

The Development Standards are not retroactive and apply only to new construction, remodeling, and changes of use. For example, an existing use with on-site parking not in compliance with the Development Standards would not be required to provide the required number of spaces at the time the Development Standards become law. However, if the existing use were to change, the new use and related parking provided would both have to be in compliance with the Development Standards. Existing uses and structures are “grandfathered in” in this manner over time.

Development in Zone 3 allows for an expansion of uses currently allowed by the Zoning Code, so long as the form of the development contributes to a sense of connection between downtown Stevenson and the City’s main tourism activity areas along the waterfront and toward Skamania Lodge.

Zone 3 includes 1) the remaining areas within the downtown area designated as High Intensity Trade in the 2013 *Stevenson Comprehensive Plan* , 2) the Rock Creek Drive corridor, 3) areas adjacent to the Columbia River, Rock Cove and Rock Creek waterfronts, and 4) other areas as designated on Page DF-2.

# Key Distinctions: Development Framework & Standards and Existing Code

Key distinctions between the proposed Development Framework and Standards and existing code are as follows:

## Zone 1 Land Uses

*Principal uses no longer permitted in Zone 1 (previously zoned C1):*

- Single-family detached dwelling
- Bank, financial institution
- Residential care facility
- Conditional uses are no longer permitted in Zone 1.

## Zone 2 Land Uses

*Principal uses no longer permitted in Zone 2 (previously zoned C1):*

- Single-family detached dwelling
- Residential care facility

*Previous conditional uses permitted as principal uses:*

- Parking structure
- Vehicle repair
- Park and playground
- Church
- Library
- Child Care Center
- Food or beverage operation with no drive-through service
- Government administration building

*Conditional uses no longer permitted in Zone 2 are:*

- Hazardous waste storage
- Communication tower in excess of fifty feet high
- Temporary emergency, construction or repair residence
- Light industrial activities

## Active Street Frontage

The requirement for 50% glazing at groundfloor street frontages along designated active streets exceeds ground level wall area requirements in the C1 Zone District. No substitutions are allowed in Zone 1.

## Lot Size/Density, Yard and Lot Coverage Requirements

Lot Size/Density, Yard and Lot Coverage requirements are superceded by the Development Standards for properties within development zones 1 and 2. Minimum lot area, width and depth requirements are eliminated except for multifamily and apartment uses. Maximum building coverage for multifamily and apartment uses is greater then that allowed in the existing code. Maximum front yard setbacks are reduced to 0' in Zone 1 except for multifamily and apartment uses.

## Parking

Parking requirements are 3 spaces per 1000 SF building floor area. For most uses, this is lower then what was previously required. In lieu of the parking requirement, property owners may choose to pay an equivalent systems development charge (to be determined) for the required spaces off-site. Requirements for multifamily and apartment uses are lowered to 1 space per unit.

## Signs

Guidelines identified in Design Guidelines - Private Development (pages PD-9 through PD-12) supercede existing ordinances.

# 2019 Interim Zoning Control Annotations

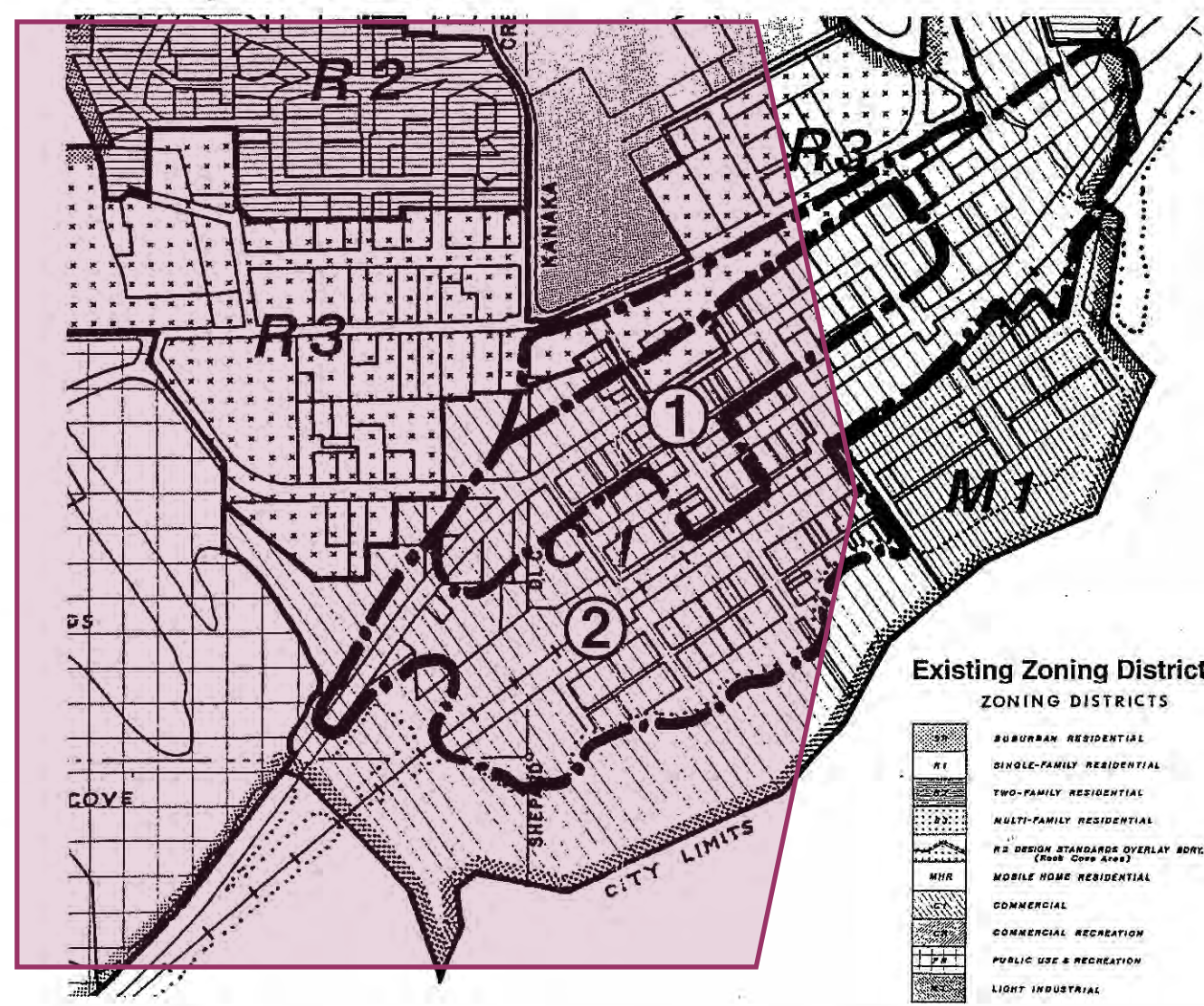
Key distinctions between the Development Framework and Standards included within these Interim Zoning Controls and the existing uses involve the transition away from Single-Family Detached Dwellings and toward mixed use development. See the table on new Pages DS 5a-c.

See also new Page DI-3

See Page PD-9 regarding sign standards.



Existing Land Use Zones and Proposed  
Development Zones



Existing Zoning Districts	
ZONING DISTRICTS	
	R2 SUBURBAN RESIDENTIAL
	R1 SINGLE-FAMILY RESIDENTIAL
	R3 TWO-FAMILY RESIDENTIAL
	M1 MULTI-FAMILY RESIDENTIAL
	R3 DESIGN STANDARDS OVERLAY BDRY (Rook Coop Area)
	MHR MOBILE HOME RESIDENTIAL
	COMMERCIAL
	COMMERCIAL RECREATION
	PUBLIC USE & RECREATION
	LIGHT INDUSTRIAL

- Proposed Development Zones
- Zone 1 - Primary Retail Corridor
  - Zone 2 - Supporting Commercial and Service Uses

The original of this map does not match that on Page DF-2. Refer to Development Zone boundaries as they currently appear on that page.



This page is kept for illustrative purposes only  
and has no intended regulatory effect. See  
Page DS-5a-c for the up-to-date Development  
Standards Matrix.

Development Standards Matrix

"-" indicates no requirement.  
Notes located on following page.

Development Zone	Land Use	Lot Size /Density				Yard			Lot Coverage Max. %	Landscape Min. % (Pervious Area Excluding Lot Coverage)	Building Height Max Stories/fl. (To Top of Roof Plate)	Parking Number Spaces Required per 1000 SF floor area Note 17
	Principal Use	Min. Lot Area	Min. Lot Width	Min. Lot Depth	Units/Acre (Max)	Min./Max. Front Yard Note 13	Min./Max. Side Note 14	Min. Rear Yard Note 15				
Zone 1 Primary Retail Corridor	Retail Sales - Note 2	-	-	-	-	0'/0'	0'/10'	0'	1	0'	3 stories/max. 50'	3
	Office - Note 3	-	-	-	-	0'/0'	0'/0'	0'	1	0'	3 stories/max. 50'	3
	MF Dwelling and Apartment - Note 3	1200 SFA/In.	-	-	24	0'/15'	0'/0'	0'	80	10% - Note 16	3 stories/max. 35'	1 space/unit
	Food/Beverage Operation - Note 4	-	-	-	-	0'/0'	0'/0'	0'	1	0'	3 stories/max. 50'	3
	Hotel - Note 5	-	-	-	-	0'/0'	0'/0'	0'	1	0'	3 stories/max. 50'	3
	Theater	-	-	-	-	0'/0'	0'/0'	0'	100%	0'	3 stories/max. 50'	3
Zone 2 Supporting Commercial and Service Uses	No Conditional Uses Permitted											
	Principal Use											
	Retail Sales - Note 6	-	-	-	-	0'-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Office	-	-	-	-	0'-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	MF Dwelling and Apartment	1200 SFA/In.	-	-	24	0'/15'	0'/0'	0'	80%	10% - Note 16	3 stories/max. 35'	1 space/unit
	Food/Beverage Operation - Note 4	-	-	-	-	0'-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Hotel - Note 5	-	-	-	-	0'-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Theater	-	-	-	-	0'-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Bank	-	-	-	-	0'-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Parking Structure - Note 18	-	-	-	-	0'-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Park and Playground	-	-	-	-	0'-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Church	-	-	-	-	0'-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Child Day Care Center	-	-	-	-	0'-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Library	-	-	-	-	0'-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Government Administration Building	-	-	-	-	0'-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Conditional Use											
	Automobile Service Station - Note 7	-	-	-	-	0'-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Parking Lot	-	-	-	-	10'- Note 19	5'- Note 19	5' Note 19	100%	20%	3 stories/max. 50'	3
	Vehicle Repair - Note 8	-	-	-	-	0'-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Fire, Police & Emergency Services Station	-	-	-	-	0'-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Hospital, Nursing Home or Convalescent Center	-	-	-	-	0'-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Utility & Communications Facilities - Note 10	-	-	-	-	0'-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Railroad Facilities - Note 11	-	-	-	-	0'-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Piers and Docks, Watercraft	-	-	-	-	-	-	-	100%	10%	3 stories/max. 50'	3



## Development Standards Matrix - Notes

- 1) Permitted as a primary use in Zone 1 and Zone 2: Canopy, marquee or awning extending into a public right-of-way.
- 2) No services permitted at street level.
- 3) Permitted at upper stories only.
- 4) No drive-through services permitted.
- 5) No motel, bed and breakfast operation, tourist home, and boarding or rooming house permitted.
- 6) Includes personal and other services, and rental operations.
- 7) Permitted provided that: 1) no major repair work is undertaken, 2) vehicles are not parked or stored overnight unless inside a fully enclosed building and 3) truck, trailer or equipment rental operations are conducted only inside a fully enclosed building.
- 8) Permitted provided that: 1) no repair work is done outside of a fully enclosed building; and that 2) vehicles are not parked or stored overnight unless inside a fully enclosed building.
- 9) Permitted provided the activity is an integral part of an on-premises principal use.
- 10) Permitted to serve the surrounding area; but exempting from the conditional use process underground cables, pipelines, vaults, and the like, and overhead cables supported on poles not over fifty feet in height.
- 11) Includes a ticket office/waiting room.
- 12) Conditional uses permitted in Zone 2 include any other use determined to be of the same general character as the principal uses or conditional uses permitted outright.
- 13) Minimum front yard setback at shoreline: 50'
- 14) 5' minimum and 0' maximum side yard setback for corner lot abutting an R District.
- 15) 20' minimum rear yard setback for 3 story building when abutting an R District; 10' minimum rear yard setback for 2 story building abutting an R District.
- 16) Balconies, terraces and roof gardens can be counted as open space.
- 17) In lieu of the parking requirement, property owners may choose to pay an equivalent systems development charge (to be determined) for the required spaces off-site.
- 18) Required active ground floor use along the street.
- 19) Required minimum setbacks to be landscaped.
- 20) R3 Zone District within proposed development zones to be Government Administration Building use only.

## 2019 Interim Zoning Control Annotations

This page is kept for illustrative purposes only and has no intended regulatory effect. See Page DS-5a-c for the up-to-date Development Standards Matrix.



Development Standards Matrix

“-” indicates no requirement.

Notes located on following page.

In cases where this table conflicts with the use allowances of SMC Tables 17.150040-1, 17.25.040-1 or 17.35.040-1, this table shall prevail.

Development Zone	Land Use	Lot Size/Density					Landscaping Minimum %	Yard			Building Height	Parking # Spaces Required per 1000 SF Floor Area
		Minimum Lot Area	Minimum Lot Width	Minimum Lot Depth	Maximum Units/Acre	Lot Coverage Maximum %		Minimum/Maximum Front Yard	Minimum/Maximum Side Yard	Minimum Rear Yard		
								Note 1	Notes 1, 2, 3	Note 4		
Zone 1—Primary Retail Corridor												
Street-Level Principal Uses												
	Retail Sales	-	-	-	-	100%	0%	0'/0'	0'/10'	0'	3 stories/ 50' maximum	3
	Food Service—Note 5	-	-	-	-	100%	0%	0'/0'	0'/10'	0'	3 stories/ 50' maximum	3
	Hotel	-	-	-	-	100%	0%	0'/0'	0'/10'	0'	3 stories/ 50' maximum	3
	Park, Playground or Outdoor Recreation Area	-	-	-	-	100%	0%	0'/0'	0'/10'	0'	3 stories/ 50' maximum	0
	Public Assembly	-	-	-	-	100%	0%	0'/0'	0'/10'	0'	3 stories/ 50' maximum	3
Street-Level Conditional Uses												
	None Permitted—Note 6	-	-	-	-	-	-	-	-	-	-	-
Upper-Story Uses—All												
	See SMC Table 17.25.040-1	See SMC Table 17.25.050-1					0%	0'/0'	0'/10'	0'	3 stories/ 50' maximum	3 or 1 space/dwelling unit
Zone 2— Supporting Commercial and Service Areas												
Street-Level Principal Uses												
	Government Administration Building	-	-	-	-	100%	0%	0'/10'	0'/10'	0'	3 stories/ 50' maximum	3
	Library	-	-	-	-	100%	0%	0'/10'	0'/10'	0'	3 stories/ 50' maximum	3
	Park, Playground or Outdoor Recreation Area	-	-	-	-	100%	0%	0'/10'	0'/10'	0'	3 stories/ 50' maximum	See SMC 17.42.090
	Townhome—Note 7	See SMC Table 17.15.050-1 & SMC 17.38.085					10%—Note 8	See SMC 17.38.085			3 stories/ 35' maximum Note 9	1 space/dwelling unit
	Multi-Family Dwelling—Note 10	1,200 SF/ Unit	-	-	24	80%	10%—Note 8	0'/10'	0'/0'	0'	3 stories/ 35' maximum Note 9	1 space/dwelling unit
	See SMC Table 17.25.040-1	See SMC Table 17.25.050-1					-	See SMC Table 17.25.060-1			3 stories/ 50' maximum	3

Development Standards Matrix

“-” indicates no requirement.  
Notes located on following page.

In cases where this table conflicts with the use allowances of SMC Tables 17.150040-1, 17.25.040-1 or 17.35.040-1, this table shall prevail.

Development Zone	Land Use	Lot Size/Density					Landscaping Minimum %	Yard			Building Height	Parking # Spaces Required per 1000 SF Floor Area
		Minimum Lot Area	Minimum Lot Width	Minimum Lot Depth	Maximum Units/Acre	Lot Coverage Maximum %		Minimum/Maximum Front Yard Note 1	Minimum/Maximum Side Yard Notes 1, 2, 3	Minimum Rear Yard Note 4		
Zone 2— Supporting Commercial and Service Areas												
Street-Level Conditional Uses												
	Single-Family Detached Dwelling—Note 11	-	-	-	-	80%	10%—Note 8	0’/10’	0’/10’	0’	3 stories/ 35’ maximum	1 space/ dwelling unit
	Manufactured Home—Note 11	-	-	-	-	80%	10%—Note 8	0’/10’	0’/10’	0’	3 stories/ 35’ maximum	1 space/ dwelling unit
	Modular Home—Note 11	-	-	-	-	80%	10%—Note 8	0’/10’	0’/10’	0’	3 stories/ 35’ maximum	1 space/ dwelling unit
	Parking Lot	-	-	-	-	100%	20%	10’/- Note 12	5’/- Note 12	5’ Note 12	3 stories/ 35’ maximum	-
	See SMC Table 17.25.040-1	-	-	-	-	100%	10%—Note 8	See SMC Table 17.25.060-1			3 stories/ 35’ maximum Note 9	3
Upper-Story Uses—Principal												
	Parking Structure—Note13	-	-	-	-	100%	10%—Note 8	0’/10’	0’/10’	0’	3 stories/ 50’ maximum	-
	See SMC Table 17.25.040-1	-	-	-	-	100%	0%	0’/10’	0’/10’	0’	3 stories/ 50’ maximum	3
Upper-Story Uses—Conditional												
	See SMC Table 17.25.040-1	-	-	-	-	100%	0%	0’/10’	0’/10’	0’	3 stories/ 50’ maximum	3
Zone 3—Future Commercial/Transitional Areas												
Principal Uses												
	See SMC Tables 17.15.040-1, 17.25.040-1, & 17.35.040-1	See SMC Tables 17.15.050-1, 17.25.050-1, & 17.35.050-1					See SMC	See SMC Tables 17.15.060-1, 17.25.060-1, & 17.35.060-1			See SMC 17.42.090	
Conditional Uses												
	Uses listed in Zone 2, provided such use contributes to a desired development type listed on Page DI-2	See Zone 2 standards as applicable to use proposed					10%—Note 8	See Zone 2 standards as applicable to use proposed			3	
	See SMC Tables 17.15.040-1, 17.25.040-1, & 17.35.040-1	See SMC Tables 17.15.050-1, 17.25.050-1, & 17.35.050-1					See SMC	See SMC Tables 17.15.060-1, 17.25.060-1, & 17.35.060-1			See SMC 17.42.090	

2019 Interim Zoning Control Annotations

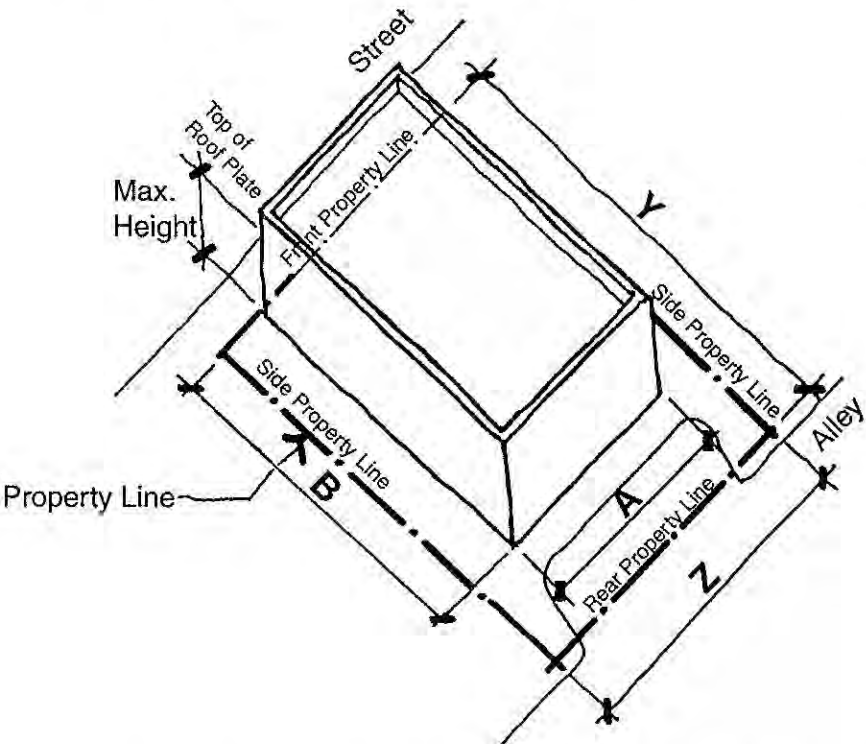
Development Standards Matrix - Notes

- 1) A site specific setback may be required from street corners for sight distance/traffic safety purposes.
- 2) 5' minimum and no maximum side yard setback for corner lot abutting an R District.
- 3) Where an alley does not provide vehicular access to the rear of a lot and where such access is necessary, the Planning Commission may allow a maximum side yard of 22' on one side. Such proposals should minimize the side yard on the opposite.
- 4) 20' minimum rear yard setback for 3 story building when abutting an R District; 10' minimum rear yard setback for 2 story building abutting an R District.
- 5) Drive-Through Food or Beverage Service prohibited.
- 6) During the effective period of these interim controls, the interpretation of uses process (SMC 17.12.020) shall not allow the establishment of conditional uses in Development Zone 1.
- 7) Townhomes proposed in Development Zone 2 are subject to the density and dimensional standards for townhomes in the R3 Multi-Family Residential District with the following exception: Minimum lot width for townhomes shall be 20'.
- 8) Balconies, terraces and roof gardens can be counted as open space.
- 9) Maximum Building Height may be increased to 50' for projects incorporating a building stepback (SMC 17.10.762) along the street frontage. The stepback shall be located at a building height which supports the pedestrian scale of its proposed location (e.g., at a similar location to the cornice lines or horizontal elements of nearby buildings). Stepback areas may be used as balconies, terraces and roof gardens.
- 10) During the effective period of these interim controls, groups of single-family detached dwellings on a single lot does not qualify as multi-family dwelling when proposed in Development Zone 2.
- 11) Projects shall conform to all design standards and applicable design guidelines located herein and shall be constructed to support adaptive reuse in the future. This includes compliance—at the time of construction—with all Americans with Disabilities Act and the International Building Code requirements applicable to retail commercial occupancy in addition to the instant residential occupancy.
- 12) Required minimum setbacks shall be landscaped to provide screening of the parking lot.



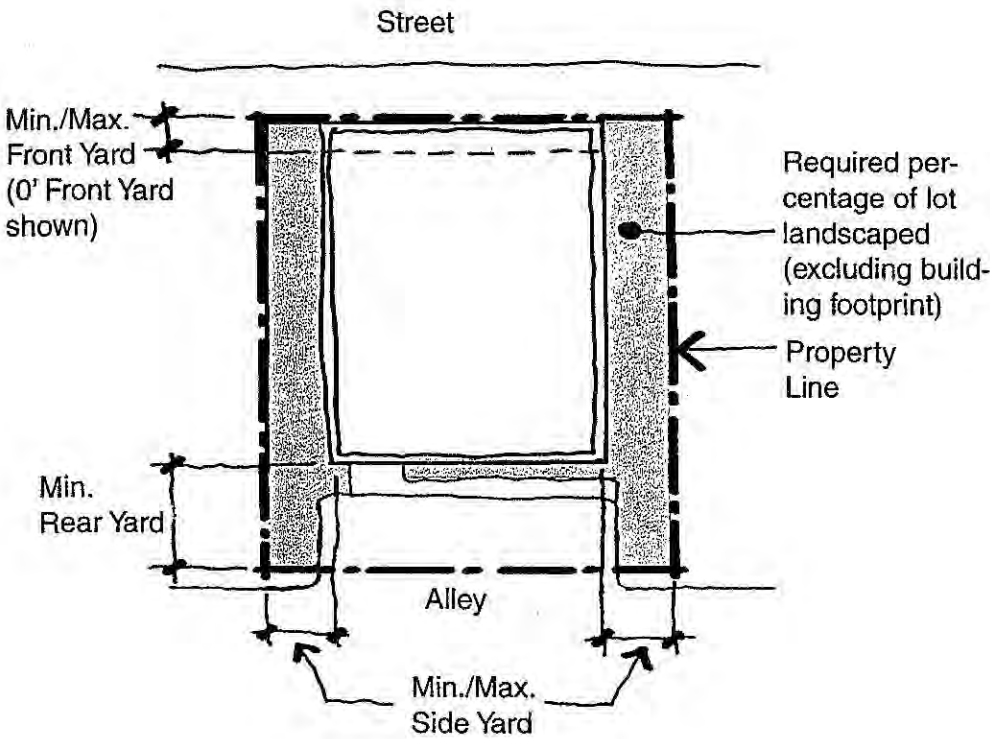
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Applying the Development Standards



Determining Lot Size/Density, Lot Coverage & Building Height

Building Footprint = A x B  
Lot Area = Y x Z  
  
Lot Coverage =  $\frac{\text{Building Footprint}}{\text{Lot Area}}$



Determining Landscape and Yard

PROJECT: STEVENSON DEVELOPMENT REVIEW CHECKLIST					2019 Interim Zoning Control Annotations
					Incentive Available?
DEVELOPMENT STANDARDS					
▪ Principal Use/Conditional Use	<input type="checkbox"/>	Does Apply	Does Not Apply	Does Comply	Does Not Comply
▪ Lot Size/Density	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Yard	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Lot Coverage	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Landscape	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Building Height	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Parking	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
DEVELOPMENT FRAMEWORK					
▪ Build-To Line	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Active Street Frontage	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Rain Protection	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
DESIGN GUIDELINES					
Public Areas					
▪ Landscaping	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Street Lighting	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Sidewalks	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Curb Extensions	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Pedestrian Street Crossings	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Street Furniture	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Railings & Fences	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Retaining Walls	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Private Development					
▪ Active Street Frontage	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Build To Line	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Rain Protection	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Building Character	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Building Form	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Windows	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Roofs	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Walls	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Entries	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Colors	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Applied Signs	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Hanging Signs	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Exterior Building Lighting	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Parking Lot Lighting	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
♦ Mixed Use (Residential Above)					<input type="checkbox"/>
♦ Affordable or Workforce Housing					<input type="checkbox"/>
♦ Mixed Use (Office Above)					<input type="checkbox"/>
♦ Waterfront Public Pedestrian Way					<input type="checkbox"/>